



*Highways Division  
Department of Infrastructure*

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## **THE MANX HIGHWAY CODE**

**Ver:- Final 05**

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## **Foreword**

### **by The Hon David Cretney MHK, Minister for Infrastructure**

The Manx Highway Code ("the Code") is essential reading for everyone.

The most vulnerable road users are pedestrians, particularly children, older or disabled people, pedal cyclists, motorcyclists and horse riders. It is important that all road users are aware of the Code and are considerate towards each other. This applies to pedestrians as much as to drivers and riders.

Many of the rules in the Code are legal requirements, and if you disobey these rules you are committing a criminal offence. You may be fined, given penalty points on your licence or be disqualified from driving, and in the most serious cases you may be sent to prison. Such rules are identified by the use of the words "**MUST**" or "**MUST NOT**" and additionally include a reference to the legislation which creates the offence.

Although failure to comply with the other rules of the Code will not of itself cause a person to be prosecuted, the Code may be used in evidence in any court proceedings under the Road Traffic Act 1985 or the Road Traffic Regulation Act 1985 to establish liability. This includes rules which use advisory wording such as "should", "should not", "do" or "do not".

The Code sets out the rules of the road as they apply in the Isle of Man. Except where otherwise stated, it does not set out the rules of the road as they apply in Great Britain, which are in various respects materially different. Consequently anyone — especially a driver — intending to visit Great Britain is recommended to obtain the GB version of the Code. It may be accessed on the GB "Highway Code" website or may be purchased from the Stationery Office in England or through any good bookseller.

Further information on driving/riding techniques can be found in "The Official DSA Guide to Driving — the essential skills" and in "The Official DSA Guide to Riding — the essential skills", both of which may be purchased in the same manner. "DSA" is a reference to the GB Driving Standards Agency.

Knowing and applying the rules contained in the Code could significantly reduce road casualties. Already good progress has been made in reducing them and I am hopeful that the Code and other measures will ensure that the trend continues. Cutting the number of deaths and injuries that occur on our roads every day is a responsibility we all share. The Code can help us to discharge that responsibility.

[Minister's signature]



# **Rules for pedestrians (1-35)**

## **General guidance**

**1**

Pavements (including any path along the side of a road) should be used if provided. Where possible, avoid being next to the kerb with your back to the traffic. If you have to step into the road, look both ways first. Always show due care and consideration for others.

**2**

If there is no pavement, keep to the right-hand side of the road so that you can see oncoming traffic. You should take extra care and —

- be prepared to walk in single file, especially on narrow roads or in poor light
- keep close to the side of the road.

It may be safer to cross the road well before a sharp right-hand bend so that oncoming traffic has a better chance of seeing you. Cross back after the bend.

**3**

Help other road users to see you. Wear or carry something light-coloured, bright or fluorescent in poor daylight conditions. When it is dark, use reflective materials (e.g. armbands, sashes, waistcoats, jackets, footwear), which can be seen by drivers using headlights up to three times as far away as non-reflective materials.



**4**

Young children should not be out alone on the pavement or road (see Rule 7). When taking children out, keep between them and the traffic and hold their hands firmly.

Strap very young children into push-chairs or use reins. When pushing a young child in a buggy, do not push the buggy into the road when checking to see if it is clear to cross, particularly from between parked vehicles.

## 5

Organised walks. Large groups of people walking together should use a pavement if available; if one is not, they should keep to the left. Look-outs should be positioned at the front and back of the group, and they should wear fluorescent clothes in daylight and reflective clothes in the dark. At night the look-out in front should show a white light and the one at the back a red light. People on the outside of large groups should also carry lights and wear reflective clothing.

## 6

Motorways in Great Britain. Pedestrians **MUST NOT** be on GB motorways or slip roads except in an emergency (see Rules 271 and 275).

[GB Road Traffic Regulation Act 1984, s. 17, Motorways Traffic (England and Wales) Regulations 1982, reg 15(1)(b), and Motorways Traffic (Scotland) Regulations 1995, reg 13]

See also Motorways (253-273) and Breakdowns and incidents (274-287).

## Crossing the road

## 7

The Green Cross Code. The advice given below on crossing the road is for all pedestrians. Children should be taught the Code and should not be allowed out alone until they can understand and use it properly. The age when they can do this is different for each child. Many children cannot judge how fast vehicles are going or how far away they are. Children learn by example, so parents and carers should always use the Code in full when out with their children. They are responsible for deciding at what age children can use it safely by themselves.

A. First find a safe place to cross and where there is space to reach the pavement on the other side. Where there is a crossing nearby, use it. It is safer to cross using a subway, a footbridge, an island, a pedestrian crossing, or where there is a crossing point controlled by a police officer, a school crossing patrol or a traffic warden. Otherwise choose a place where you can see clearly in all directions. Try to avoid crossing between parked cars (see Rule 14), on a blind bend, or close to the brow of a hill. Move to a space where drivers and riders can see you clearly. Do not cross the road diagonally.



**Rule 7: Look all around and listen for traffic before crossing**

B. Stop just before you get to the kerb, where you can see if anything is coming. Do not get too close to the traffic. If there's no pavement, keep back from the edge of the road but make sure you can still see approaching traffic.

C. Look all around for traffic. Traffic could come from any direction. Listen as well, because you can sometimes hear traffic before you see it.

D. If traffic is coming, let it pass. Look all around again and listen. Do not cross until there is a safe gap in the traffic and you are certain that there is plenty of time. Remember, even if traffic is a long way off, it may be approaching very quickly.

E. When it is safe, go straight across the road — do not run. Keep looking and listening for traffic while you cross, in case there is any traffic you did not see, or in case other traffic appears suddenly. Look out for pedal cyclists and motorcyclists travelling between lanes of traffic. Do not walk diagonally across the road.

**8**

At a junction. When crossing the road, look out for traffic turning into the road, especially from behind you. If you have started crossing and traffic wants to turn into the road, you have priority and they should give way (see Rule 170).

See also Using the road (159-203).

**9**

Pedestrian Safety Barriers. Where there are barriers, cross the road only at the gaps provided for pedestrians. Do not climb over the barriers or walk between them and the road.

**10**

Tactile paving. Raised surfaces that can be felt underfoot provide warning and guidance to blind or partially sighted people. The two most common surfaces are

raised studs and rounded raised bars. A series of raised studs are used at crossing points with a dropped kerb. A series of rounded raised bars are used at level crossings, at the top and bottom of steps and at some other hazards.

## 11

One-way streets. Check which way the traffic is moving. Do not cross until it is safe to do so without stopping.

## 12

Bus and cycle lanes in Great Britain. Take care when crossing these lanes as traffic may be moving faster than in the other lanes or in the opposite direction to the rest of the traffic.

## 13

Routes in Great Britain shared with pedal cyclists. Some cycle tracks in Great Britain run alongside footpaths or pavements, using a segregating feature to separate pedal cyclists from people on foot. Such segregated routes may also incorporate short lengths of tactile paving to help visually impaired people stay on the correct side. On the pedestrian side this will comprise a series of flat-topped bars running across the direction of travel (ladder pattern). On the cyclist side the same bars are orientated in the direction of travel (tramline pattern). Not all routes which are shared with pedal cyclists are segregated. Take extra care where this is so (see Rule 62).

See also Rules for pedal cyclists (59-82).

## 14

Parked vehicles. If you have to cross between parked vehicles, use the outside edges of the vehicles as if they were the kerb. Stop there and make sure that you can see all around and that the traffic can see you. Make sure there is a gap between any parked vehicles on the other side, so you can reach the pavement. Never cross the road in front of, or behind, any vehicle with its engine running, especially a large vehicle, as the driver may not be able to see you.

## 15

Reversing vehicles. Never cross behind a vehicle which is reversing, showing white reversing lights or sounding a warning.

## 16

Moving vehicles. You **MUST NOT** get on to or hold on to a moving vehicle.

[Road Traffic Act 1985, s. 23]

## 17

At night. Wear something reflective to make it easier for others to see you (see Rule 3). If there is no pedestrian crossing nearby, cross the road near a street light so that traffic can see you more easily.

## Crossings

## 18

When using any type of crossing you should —

- always check that the traffic has stopped before you start to cross or push a pram on to a crossing
- always cross between the studs or over the zebra markings. Do not cross at the side of the crossing or on the zig-zag lines, as it can be dangerous.

You **MUST NOT** loiter on any type of crossing.

[Road Traffic Regulation Act 1985, s. 7(1) and (4)]

## 19

Zebra crossings. Give traffic plenty of time to see you and to stop before you start to cross. Vehicles will need more time when the road is slippery. Wait until traffic has stopped from both directions or the road is clear before crossing. Remember that traffic does not have to stop until someone has moved on to the crossing. Keep looking both ways, and listening, in case a driver or rider has not seen you and attempts to overtake a vehicle that has stopped.



Rule 19: Zebra crossings have flashing beacons

## 20

Where there is an island in the middle of a zebra crossing, wait on the island and follow Rule 19 before you cross the second half of the road – it is a separate crossing.



**Rule 20: Zebra crossings with a central island are two separate crossings**

## 21

At traffic lights. There may be special signals for pedestrians. You should only start to cross the road when the green figure shows. If you have started to cross the road and the green figure goes out, you should still have time to reach the other side, but do not delay. If no pedestrian signals have been provided, watch carefully and do not cross until the traffic lights are red and the traffic has stopped. Keep looking and check for traffic that may be turning the corner. Remember that traffic lights may let traffic move in some lanes while traffic in other lanes has stopped.



**Rule 21: At traffic lights, puffin and pelican crossings**  
\* At pelican crossings only

## 22

Pelican crossings. These are signal-controlled crossings operated by pedestrians. Push the control button to activate the traffic signals. When the red figure shows, do

not cross. When a steady green figure shows, check the traffic has stopped and then cross with care. When the green figure begins to flash you should not start to cross. If you have already started you should have time to finish crossing safely.

## 23

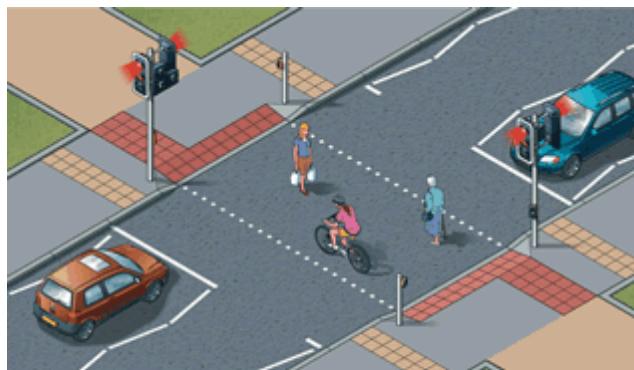
Puffin crossings differ from pelican crossings as the red and green figures are above the control box on your side of the road and there is no flashing green figure phase. Press the button and wait for the green figure to show.

## 24

When the road is congested, traffic on your side of the road may be forced to stop even though their lights are green. Traffic may still be moving on the other side of the road, so press the button and wait for the signal to cross.

## 25

Toucan crossings are light-controlled crossings which allow pedal cyclists and pedestrians to share crossing space and cross at the same time. They are push-button operated. Pedestrians and pedal cyclists will see the green signal together. Pedal cyclists are permitted to ride across. At present there are few in the Isle of Man but they can be more frequently encountered in Great Britain.



**Rule 25: Toucan crossings can be used by both cyclists and pedestrians**

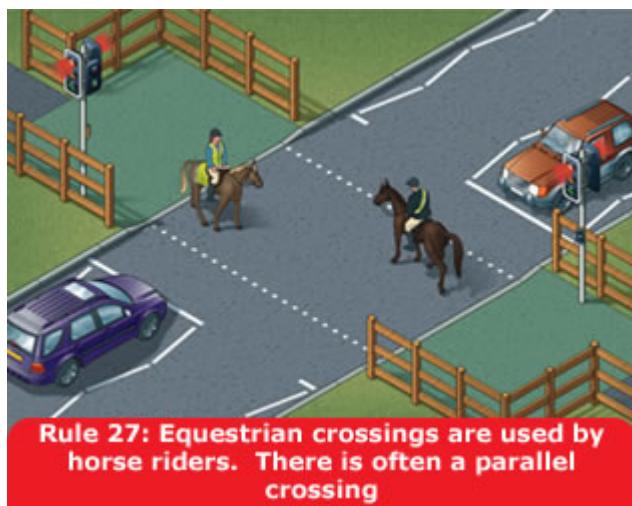
## 26

At some crossings there is a bleeping sound to indicate to blind or partially sighted people when the steady green figure is showing, and there may be a tactile signal to help deafblind people.

## 27

Equestrian crossings are for horse riders. They have pavement barriers, wider crossing spaces, horse and rider figures in the light panels and either two sets of

controls (one higher) or just one higher control panel. At present there are none in the Isle of Man but they can be encountered in Great Britain.



## 28

"Staggered" pelican or puffin crossings. When the crossings on each side of the central refuge are not in line, they are two separate crossings. On reaching the central island, press the button again and wait for a steady green figure.



## 29

Crossings controlled by an authorised person. Do not cross the road unless you are signalled to do so by a police officer, traffic warden or school crossing patrol. Always cross in front of them.

## 30

Where there are no controlled crossing points available it is advisable to cross where there is an island in the middle of the road. Use the Green Cross Code (see Rule 7)

to cross to the island and then stop and use it again to cross the second half of the road.

## **Situations needing extra care**

### **31**

Emergency vehicles. If an ambulance, fire engine, police or other emergency vehicle approaches using flashing blue lights, headlights and/or sirens, keep off the road.

### **32**

Buses. Get on or off a bus only when it has stopped to allow you to do so. Watch out for cyclists when you are getting off. Never cross the road directly behind or in front of a bus. Wait until it has moved off and you can see clearly in both directions.

### **33**

Tramways. Cross at designated crossings, where provided. Elsewhere treat trams as you would other road vehicles and look both ways along the track before crossing. Do not walk along the track as trams may come up behind you and they cannot steer to avoid you.

### **34**

Railway level crossings. You **MUST NOT** cross or pass a stop line when the red lights or signs show. Also do not cross if an alarm is sounding or the barriers are being lowered. In Great Britain the tone of the alarm may change if another train is approaching. If there are no lights, alarms or barriers, stop, look both ways and listen before crossing. A tactile surface comprising rounded bars running across the direction of pedestrian travel may be installed on the footpath approaching a level crossing to warn visually impaired people of its presence. The tactile surface should extend across the full width of the footway and should be located at an appropriate distance from the barrier or projected line of the barrier.

[Traffic Signs Regulations 2002, reg 52]

### **35**

Street and pavement repairs. A pavement may be closed temporarily because it is not safe to use. Take extra care if you are directed to walk in, or to cross, the road.

# **Rules for users of powered wheelchairs and mobility scooters (36-46)**

(being very low-speed classes of invalid carriages to which  
The Use of Invalid Carriages on Highways Regulations 1996 apply)

## **36**

There is one class of manual wheelchairs (called Class 1 invalid carriages) and two classes (2 and 3) of powered wheelchairs and powered mobility scooters. Manual wheelchairs and Class 2 vehicles are those with an upper speed limit of 4 mph and are designed to be used on pavements. Class 3 vehicles are those with an upper speed limit of 8 mph and are equipped to be used on the road as well as on the pavement.

## **37**

When you are on the road, you should obey the guidance and rules for other vehicles; when on the pavement, you should follow the guidance and rules for pedestrians. In both cases it is an offence to drive dangerously, furiously, carelessly or inconsiderately.

[Road Traffic Act 1985, ss. 2 and 3, and Petty Sessions and Summary Jurisdiction Act 1927, s. 66(4)]

### **On pavements**

## **38**

Pavements are safer than roads and should be used when available. You should give pedestrians priority and show consideration for other pavement users, particularly those with a hearing or visual impairment who may not be aware that you are there.

## **39**

Powered wheelchairs and mobility scooters **MUST NOT** travel faster than 4 mph on pavements or in pedestrian areas. You may need to reduce your speed to adjust to other pavement users who may not be able to move out of your way quickly enough or where the pavement is too narrow.

[The Use of Invalid Carriages on Highways Regulations 1996 (UICHR), regs 2, 6 and 10(1)(c)(ii)]

## **40**

When moving off the pavement on to the road, you should take special care. Before moving off, always look round and make sure it's safe to join the traffic. Always try

to use dropped kerbs when moving off the pavement, even if this means travelling further to locate one. If you have to climb or descend a kerb, always approach it at right angles and don't try to negotiate a kerb higher than the vehicle manufacturer's recommendations.

## On the road

### 41

You should take care when travelling on the road as you may be travelling more slowly than other traffic, given that your machine is restricted to 8 mph and may be less visible.

### 42

When on the road, Class 3 vehicles should travel in the direction of the traffic. Class 2 users should always use the pavement when it is available. When there is no pavement, you should use caution when on the road. Class 2 users should, where possible, travel in the direction of the traffic. If you are travelling at night when lights **MUST** be used, you should travel in the direction of the traffic to avoid confusing other road users.

[UICHR, regs 8 and 15]

### 43

You **MUST** follow the same rules about using lights, indicators and horns as for other road vehicles, if your vehicle is fitted with them. At night, lights **MUST** be used. Be aware that other road users may not see you and you should make yourself more visible — both in the daytime and at dusk, for instance by wearing a reflective jacket or reflective strips on the back of the vehicle.

[UICHR, regs 8, 15 and 18]

### 44

Take extra care at road junctions. When going straight ahead, check to make sure there are no vehicles about to cross your path from the left or the right, or overtaking you and turning left. There are several options for dealing with right turns, especially turning from a major road. If moving into the middle of the road is difficult or dangerous, you can —

- stop on the left-hand side of the road and wait for a safe gap in the traffic
- negotiate the turn as a pedestrian, i.e. travel along the pavement and cross the road between pavements where it is safe to do so. Class 3 users should switch the vehicle to the lower speed limit when on pavements.

If the junction is too hazardous, it may be worth considering an alternative route. Similarly, when negotiating major roundabouts (i.e. with two or more lanes) it may be safer for you to use the pavement or find a route which avoids the roundabout altogether.

## 45

All normal parking restrictions should be observed. Your vehicle should not be left unattended if it causes an obstruction to other pedestrians — especially those in wheelchairs. Parking concessions provided under the Blue Badge scheme will apply to those vehicles displaying a valid badge.

## 46

In Great Britain these vehicles **MUST NOT** be used on motorways (see Rule 253); **MUST NOT** be used on unrestricted dual carriageways without a flashing amber beacon; should not be used on such carriageways which have a speed limit exceeding 50 mph; and should not be used on other dual carriageways without such a flashing beacon.

[GB Road Traffic Regulation Act 1984, s. 17(2) and (3), and Road Vehicles Lighting Regulations 1989, regs 17(1) and 26]

# Rules about animals (47-58)

## Horse-drawn vehicles

### 47

Horse-drawn vehicles used on the highway should be operated and maintained in accordance with standards set out in the GB Department for Transport's Code of Practice for Horse-Drawn Vehicles. The Code lays down the requirements for a road driving assessment and includes a comprehensive list of safety checks to ensure that a carriage and its fittings are safe and in good working order. The Code is available from the GB Department for Transport, Transport Technology and Standards Division 6, 2nd Floor, Great Minster House, 76 Marsham Street, London SW1P 4DR (telephone 0207 944 2078).

### 48

Safety equipment and clothing. All horse-drawn vehicles should have two red rear reflectors. It is safer not to drive at night, but if you do, a light showing white to the front and red to the rear should be fitted.

## Horse riders

**49**

Safety equipment. Children under the age of 14 should wear a helmet which meets the same requirements as apply in Great Britain under regulation 4 of the Horses (Protective Headgear for Young Riders) Regulations 1992. It should be fastened securely. Other riders should also follow this advice, but it does not apply to persons of the Sikh religion while a turban is being worn.

**50**

Other clothing. You should wear —

- boots or shoes with hard soles and heels
- light-coloured or fluorescent clothing in daylight
- reflective clothing if you have to ride at night or in poor visibility.



**51**

At night. It is safer not to ride on the road at night or in poor visibility, but if you do, make sure you wear reflective clothing and your horse has reflective bands above the fetlock joints. A light which shows white to the front and red to the rear should be fitted, with a band, to the rider's right arm and/or leg/riding boot. If you are leading a horse at night, carry a light in your right hand, showing white to the front and red to the rear, and wear reflective clothing on both yourself and your horse. It is strongly recommended that a fluorescent/reflective tail guard be also worn by your horse.

## Riding

**52**

Before you take a horse on to a road, you should ensure that —

- all tack fits well and is in good condition
- you can control the horse.

Always ride with other, less nervous horses if you think that your horse will be nervous of traffic. Never ride a horse without both a saddle and bridle.

## 53

Before riding off or turning, look behind you to make sure it is safe, and then give a clear arm signal.

When riding on the road you should —

- keep to the left
- keep both hands on the reins unless you are signalling
- keep both feet in the stirrups
- not carry another person
- not carry anything which might affect your balance or get tangled up with the reins
- keep a horse you are leading to your left
- move in the direction of the traffic flow in a one-way street
- never ride more than two abreast, and ride in single file on narrow or busy roads and when riding round bends.

## 54

You **MUST NOT** take a horse on to a pavement, and you should not take a horse on to a footpath, cycle-path or cycle track. Use a bridle-path where possible. Equestrian crossings may be provided in Great Britain for horse riders to cross the road and, if there, you should use these where available (see Rule 27). You should dismount at level crossings in Great Britain where a "horse rider dismount" sign is displayed.

[Petty Sessions and Summary Jurisdiction Act 1927, s. 63(14)]

## 55

Avoid roundabouts wherever possible. If you use them you should —

- keep to the left and watch out for vehicles crossing your path to leave or join the roundabout
- signal right when riding across exits to show you are not leaving
- signal left just before you leave the roundabout.

## **Other animals**

**56**

Dogs. Do not let a dog out on to the road on its own. Keep it on a short lead when walking on the pavement, road, or path shared with cyclists or horse riders.

**57**

When in a vehicle, make sure dogs or other animals are suitably restrained so they cannot distract you while you are driving or injure you, or themselves, if you stop quickly. A seat-belt harness, pet carrier, dog cage or dog guard are ways of restraining animals in cars.

**58**

Animals being herded. These should be kept under control at all times. You should, if possible, send another person along the road in front to warn other road users, especially at a bend or the brow of a hill. It is safer not to move animals after dark, but if you do, then wear reflective clothing and ensure that lights are carried (white at the front and red at the rear of the herd).

## **Rules for pedal cyclists (59-82)**

These rules are in addition to those in the following sections, which apply to all vehicles (except the motorway section). See also Annex 1: "You and your pedal cycle".

**59**

Clothing. You should wear —

- a cycle helmet which is the correct size and is securely fastened
- appropriate clothes for cycling. Avoid clothes which may get tangled in the chain or a wheel or may obscure your lights
- light-coloured or fluorescent clothing which helps other road users to see you in daylight and poor light
- reflective clothing and/or accessories (belt, arm or ankle bands) in the dark.



Rule 59: Help yourself to be seen

## 60

At night your cycle **MUST** have white front and red rear lights lit. It **MUST** also be fitted with a red rear reflector (and amber pedal reflectors, if manufactured on or after 1/10/85). White front reflectors and spoke reflectors will also help you to be seen. Flashing lights are permitted but it is recommended that pedal cyclists who are riding in areas without street lighting use a steady front lamp.

[Road Vehicles Lighting Regulations 1989, regs 13, 18 and 24, as they have effect with modifications in the Isle of Man by virtue of the Road Vehicles Lighting (Application) Regulations 1998 (as amended)]

## 61

Cycle routes and other facilities, preponderantly in Great Britain. Use cycle routes, advanced stop lines, cycle boxes and toucan crossings unless at the time it is unsafe to do so. Use of these facilities is not compulsory and will depend on your experience and skills, but they can make your journey safer.

## 62

Cycle tracks in Great Britain. These are normally located away from the road, but may occasionally be found alongside footpaths or pavements. Pedal cyclists and pedestrians may be segregated or they may share the same space (unsegregated). When using segregated tracks, you **MUST** keep to the side intended for pedal cyclists as the pedestrian side remains a pavement or footpath. Take care when passing pedestrians, especially children or older or disabled people, and allow them plenty of room. Always be prepared to slow down and stop if necessary. Take care near road junctions as you may have difficulty seeing other road users, who might not notice you.

[GB Highway Act 1835, s. 72]

## **63**

Cycle lanes in Great Britain. These are marked by a white line (which may be broken) along the carriageway (see Rule 140). Keep within the lane when practicable. When leaving a cycle lane, check before pulling out that it is safe to do so and signal your intention clearly to other road users. Use of cycle lanes is not compulsory and will depend on your experience and skills, but they can make your journey safer. See also Multi-lane carriageways (133-143).

## **64**

You **MUST NOT** cycle on a pavement.

[Road Traffic Act 1985, s. 30(1)]

## **65**

Bus Lanes in Great Britain. Most bus lanes there may be used by cyclists as indicated on signs. Watch out for people getting on or off a bus. Be very careful when overtaking a bus or leaving a bus lane as you will be entering a busier traffic flow.

Do not pass between the kerb and a bus when it is at a stop.

## **66**

You should —

- keep both hands on the handlebars except when signalling or changing gear
- keep both feet on the pedals
- never ride more than two abreast, and ride in single file on narrow or busy roads and when riding round bends
- not ride close behind another vehicle
- not carry anything which will affect your balance or may get tangled up with your wheels or chain
- fit a bell
- be considerate of other road users, particularly blind and partially sighted pedestrians. Let them know you are there when necessary, for example by ringing your bell if you have one.

## **67**

You should —

- look all around to make sure it is safe before moving away from the kerb, turning or manoeuvring. Give a clear signal to show other road users what you intend to do (see Annex 2: "Signals to other road users")
- look well ahead for obstructions in the road such as drains, pot-holes and parked vehicles so that you do not have to swerve suddenly to avoid them

- leave plenty of room when passing parked vehicles and watch out for doors being opened or pedestrians stepping into your path
- be aware of traffic coming up behind you
- take extra care near road humps, narrowings and other traffic calming features
- take care when overtaking (see Rules 162-169).

## 68

You **MUST NOT** —

- carry a passenger unless your cycle has been built or adapted to carry one
- hold on to a moving motor vehicle or trailer
- ride in a dangerous, careless or inconsiderate manner
- ride when under the influence of drink or drugs, including medicine.

[Road Traffic Act 1985, sections 11 to 13, 15 and 23]

## 69

You **MUST** obey all traffic signs and traffic light signals.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, reg 10(1)]

## 70

When parking your pedal cycle —

- find a conspicuous location where it can be seen by passers-by
- use cycle stands or other cycle parking facilities wherever possible
- do not leave it where it would cause an obstruction or hazard to other road users
- secure it well so that it will not fall over and become an obstruction or hazard.

## 71

You **MUST NOT** cross the stop line when the traffic lights are red. Some junctions in Great Britain have an advanced stop line to enable you to wait and position yourself ahead of other traffic (see Rule 178).

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10 and 36(1))]

## Road junctions

**72**

On the left. When approaching a junction on the left, watch out for vehicles turning in front of you out of or into the side road. Just before you turn, check for undertaking pedal cyclists or motorcyclists. Do not ride on the inside of vehicles signalling or slowing down to turn left.

**73**

Pay particular attention to long vehicles which need a lot of room to manoeuvre at corners. Be aware that drivers may not see you. They may have to move over to the right before turning left. Wait until they have completed the manoeuvre because the rear wheels come very close to the kerb while turning. Do not be tempted to ride in the space between them and the kerb.

**74**

On the right. If you are turning right, check the traffic to ensure it is safe, and then signal and move to the centre of the road. Wait until there is a safe gap in the oncoming traffic and give a final look before completing the turn. It may be safer to wait on the left until there is a safe gap or to dismount and push your cycle across the road.

**75**

Dual carriageways, preponderantly in Great Britain. Remember that traffic on most dual carriageways moves quickly. When crossing wait for a safe gap and cross each carriageway in turn. Take extra care when crossing slip roads.

**76**

Full details about the correct procedure at roundabouts are contained in Rules 184–190. Roundabouts can be hazardous and should be approached with care.

## Roundabouts

**77**

You may feel safer walking your pedal cycle round on the pavement or verge. If you decide to ride round, keeping to the left-hand lane, you should —

- be aware that drivers may not easily see you
- take extra care when cycling across exits. You may need to signal right to show you are not leaving the roundabout

- watch out for vehicles crossing your path to leave or join the roundabout.

## 78

Give plenty of room to long vehicles on the roundabout as they need more space to manoeuvre. Do not ride in the space they need to get round the roundabout. It may be safer to wait until they have cleared the roundabout.

### Crossing the road

## 79

When using a pedestrian crossing, do not ride. Dismount and wheel your cycle across. Do not use equestrian crossings in Great Britain as they are for horse riders only.

## 80

Toucan crossings. These are light-controlled crossings which allow pedal cyclists and pedestrians to share crossing space and cross at the same time. They are push-button operated. Pedestrians and pedal cyclists will see the green signal together. Pedal cyclists are permitted to ride across.

## 81

GB cycle-only crossings. Cycle tracks in Great Britain on opposite sides of the road may be linked by signalled crossings. You may ride across but you **MUST NOT** cross until the green cycle symbol is showing.

[GB Road Traffic Act 1988, s. 36, and Traffic Signs Regulations 2002, regs 33(2) and 36(1))]

## 82

Level crossings/tramways. Take extra care when crossing the tracks (see Rule 306). In Great Britain you should dismount at level crossings where a "cyclist dismount" sign is displayed. See also Road works, level crossings and tramways (288-308).

## Rules for motorcyclists (83-88)

These Rules are in addition to those in the following sections, which apply to all vehicles. See also Annex 3: "Motorcycle licence requirements".

## General

### 83

On all journeys the driver and pillion passenger on a motorcycle, scooter or moped **MUST** wear a protective helmet. This does not apply to a follower of the Sikh religion while wearing a turban. Helmets **MUST** comply with the Regulations below and they **MUST** be strapped to the head securely. Riders and passengers of motor tricycles and quadricycles, also called quadbikes, should also wear a protective helmet. Before each journey check that your helmet visor is clean and in good condition.

[Road Traffic Act 1985, s. 24, and Motor Cycles (Protective Helmets and Visors) Regulations 2000]

### 84

It is also advisable to wear eye protectors. Scratched or poorly fitting eye protectors can limit your view when riding, particularly in bright sunshine and the hours of darkness. Consider wearing ear protection. Strong boots, gloves and suitable clothing may help to protect you if you are involved in a collision.

### 85

You **MUST NOT** carry more than one pillion passenger, who **MUST** sit astride the machine on a proper seat. He or she **MUST** face forward with both feet on the footrests, and you should also do so. You should not carry a pillion passenger unless your motor cycle is designed to do so. Provisional licence holders **MUST NOT** carry a pillion passenger.

[Road Traffic Act 1985, s. 10, Road Vehicles (Maintenance and Use) Regulations 2012, reg 59, and Driving Licences and Tests Regulations 2007, reg 8(2)(b)]

### 86

Daylight riding. Make yourself as visible as possible from the side as well as the front and rear. You could wear a light or brightly coloured helmet and fluorescent clothing or strips. Dipped headlights, even in good daylight, may also make you more conspicuous. However, be aware that other vehicle drivers may still not have seen you or judged your distance or speed correctly, especially at junctions.



**Rule 86: Help yourself to be seen**

## 87

Riding in the dark. Wear reflective clothing or strips to improve your visibility in the dark. These reflect light from the headlamps of other vehicles, making you visible from a longer distance. See Rules 113–116 for lighting requirements.

## 88

Manoeuvring or cornering. You should be aware of what is behind and to the sides before manoeuvring. Look behind you; use mirrors if they are fitted. When in traffic queues, look out for pedestrians crossing between vehicles and vehicles emerging from junctions or changing lanes. Position yourself so that drivers in front can see you in their mirrors. Additionally, when filtering in slow-moving traffic, take care and keep your speed low. When cornering, ensure that not only your machine but also your body stay to the left of the centre line of the road or, if you are riding in a lane, to the left of the right-hand side of the lane.

Remember: Observation — Signal — Manoeuvre.

## **Rules for drivers and motorcyclists (89-102)**

### 89

Vehicle condition. You **MUST** ensure your vehicle and trailer comply with the full requirements of —

- the Road Vehicles (Maintenance and Use) Regulations 2012
- the Road Vehicles (Construction, Equipment and Weights) Regulations 2012
- the Road Vehicles Lighting Regulations 1989 as they have effect with modifications in the Isle of Man by virtue of the Road Vehicles Lighting (Application) Regulations 1998 (as amended).

See also Annex 4: "The road user and the law".

## **Fitness to drive**

**90**

Make sure that you are fit to drive. You **MUST** report to the Licensing Office of the Department of Infrastructure any health condition likely to affect your driving.

[Road Traffic Act 1985, Schedule 3, paras 4 and 5A]

**91**

Driving when you are tired greatly increases your risk of collision. To minimise this risk —

- make sure you are fit to drive. Do not begin a journey if you are tired. Get a good night's sleep before embarking on a long journey
- avoid undertaking long journeys between midnight and 6 am, when natural alertness is at a minimum
- plan your journey to take sufficient breaks. A minimum break of at least 15 minutes after every two hours of driving is recommended
- if you feel at all sleepy, stop in a safe place. Do not stop on the hard shoulder of a motorway in Great Britain
- the most effective ways to counter sleepiness are to drink, for example, two cups of caffeinated coffee and to take a short nap (at least 15 minutes).

**92**

Vision. You **MUST** be able to read in good daylight a registration mark fixed to a motor vehicle and containing —

- characters 79 mm high and 57 mm wide when viewed from a distance of 20.5 metres, or
- characters 79 mm high and 50 mm wide when viewed from a distance of 20 metres, or
- characters 79 mm high and 44 mm wide when viewed from a distance of 19.5 metres.

If you need to wear glasses or contact lenses to do this, you **MUST** wear them at all times while driving. A police constable has power to report to the Licensing Office of the Department of Infrastructure any driver who is suspected of having defective eyesight. The driver will be required by the Licensing Office to undergo an eyesight test.

[Driving Licences and Tests Regulations 2007, reg 14(1)(g), and Road Traffic Act 1985, Schedule 3, para 9]

## **93**

Slow down, and if necessary stop, if you are dazzled by bright sunlight.

## **94**

At night or in poor visibility do not use tinted glasses, lenses or visors if they restrict your vision.

### **Alcohol and drugs**

## **95**

Do not drink and drive as it will seriously affect your judgement and abilities. You **MUST NOT** drive with a breath alcohol level higher than 35 micrograms of alcohol in 100 millilitres of breath, a blood alcohol level of more than 80 milligrams of alcohol in 100 millilitres of blood, or a urine alcohol level of more than 107 milligrams of alcohol in 100 millilitres of urine. Alcohol will —

- give a false sense of confidence
- reduce co-ordination and slow down reactions
- affect judgement of speed, distance and risk
- reduce your driving ability, even if you're below the legal limit
- take time to leave your body; you may be unfit to drive in the evening after drinking at lunchtime, or in the morning after drinking the previous evening.

The best solution is not to drink at all when planning to drive because any amount of alcohol affects your ability to drive safely. If you are going to drink, arrange another means of transport.

[Road Traffic Act 1985, ss. 5A and 7E(2)]

## **96**

You **MUST NOT** drive under the influence of drugs or medicine. Check the instructions or ask your doctor or pharmacist. Using illegal drugs is highly dangerous. Never take them if you intend to drive; the effects are unpredictable, but can be even more severe than alcohol and may result in fatal or serious road crashes.

[Road Traffic Act 1985, s. 5]

## **97**

Before setting off. You should ensure that —

- you have planned your route and allowed sufficient time
- clothing and footwear do not prevent you from using the controls in the correct manner
- you know where all the controls are and how to use them before you need them. Not all vehicles are the same; do not wait until it is too late to find out
- your mirrors and seat are adjusted correctly to ensure comfort, full control and maximum vision
- head restraints are properly adjusted to reduce the risk of neck and spine injuries in the event of a collision
- you have sufficient fuel before commencing your journey, especially if it includes motorway driving. It can be dangerous to lose power when driving in traffic
- ensure your vehicle is legal and roadworthy
- switch off your mobile phone.



## 98

Vehicle towing and loading. As a driver —

- you **MUST NOT** tow a trailer of greater weight than your licence permits
- you **MUST NOT** overload your vehicle or trailer. Comply with the manufacturer's recommendations, but do not exceed any relevant lawfully prescribed weights
- you **MUST** secure your load and it **MUST NOT** stick out dangerously. Make sure any heavy or sharp objects and any animals are secured safely. If there is a collision, they might hit someone inside the vehicle and cause serious injury
- you should properly distribute the weight in your caravan or trailer with heavy items mainly over the axle(s) and ensure a downward load on the tow ball. Do not exceed the manufacturer's recommended weight and tow ball load, but any relevant lawfully prescribed weight takes precedence. This should avoid the possibility of swerving or snaking and going out of control. If this

- does happen, ease off the accelerator and reduce speed gently to regain control
- carrying a load or pulling a trailer may require you to adjust the headlights.

In the event of a breakdown, be aware that towing a vehicle on a tow rope is potentially dangerous. The vehicle should be towed only so far as to clear the carriageway and not for long distances. You should consider professional recovery.

[Road Traffic Act 1985, Schedule 3, para 2(4)(a), and Road Vehicles (Maintenance and Use) Regulations 2012, reg 14]

## **Seat belts and child restraints**

**99**

Seat-belt requirements. The tables below summarise the legal requirements for the wearing of seat belts or child restraints in motor vehicles. It is an offence to contravene them.

### **MODERN PASSENGER CARS**

(Seat belts are required to be fitted to the front seats in cars first registered from 1st January 1965 and to the rear seats in cars first registered from 1st April 1987)

	<b><i>Front Seat</i></b>	<b><i>Rear Seat</i></b>	<b><i>Responsibility</i></b>
Driver	Seat belt must be worn		Driver
Adult passenger i.e. 14 years or over	Seat belt must be worn	Seat belt must be worn	Passenger
Child under 4 years	Correct child restraint must be worn. (A rear facing child restraint must not be used in a seat protected by a frontal airbag unless the airbag has been de-activated)	Correct child restraint must be worn (A rear facing child restraint must not be used in a seat protected by a frontal airbag unless the airbag has been de-activated)	Driver
Child from 4th birthday who is under the age of 12 years and under a height of 1.35 metres (4 ft 5 ins)	Correct child restraint must be worn	(a) Correct child restraint must be worn unless paragraph (b) applies  (b) If no correct child restraint is available in the front or rear, an adult seat belt may be worn if the driver can prove that the child, because of an unexpected necessity, is being carried a distance of 5 miles or less	Driver

Child at or over 1.35 metres (4ft 5ins) or who is aged 12 or 13 years	Correct child restraint or adult belt must be worn	Correct child restraint or adult belt must be worn	Driver
Child under the age of 3 years in a taxi or private-hire car	Correct child restraint must be worn (A rear facing child restraint must not be used in a seat protected by a frontal airbag unless the airbag has been de-activated)	No seat belt or child restraint need be worn (A rear facing child restraint must not be used in a seat protected by a frontal airbag unless the airbag has been de-activated)	Driver
Child of 3 years or more in a taxi or private-hire car	(a) Correct child restraint must be worn unless paragraph (b) applies  (b) Instead of a child restraint, an adult belt may be worn by a child at or over 1.35 metres (4ft 5ins) or who is aged 12 or 13 years	Either a correct child restraint or an adult belt must be worn	Driver
Adult passenger in a taxi or private-hire car	Seat belt must be worn	Seat belt must be worn	Passenger

## **BUSES, COACHES, GOODS VEHICLES AND MINIBUSES**

	<b>Front Seat</b>	<b>Rear Seat</b>	<b>Responsibility</b>
Driver	Seat belt must be worn if fitted		Driver
Adult passenger i.e. 14 years or over	Seat belt must be worn if an available seat in the front or rear is fitted with a seat belt	Seat belt must be worn if an available seat in the front or rear is fitted with a seat belt	Passenger
Child under 4 years	Correct child restraint must be worn in coaches, goods vehicles and minibuses (A rear facing child restraint must not be used in a seat protected by a frontal airbag unless the airbag has been de-activated)	Correct child restraint must be worn in goods vehicles and minibuses (A rear facing child restraint must not be used in a seat protected by a frontal airbag unless the airbag has been de-activated)	Driver

Child from 4th birthday who is under the age of 12 years and under a height of 1.35 metres (4 ft 5 ins)	Correct child restraint must be worn in coaches, goods vehicles and minibuses	(a) Correct child restraint must be used in goods vehicles and minibuses unless paragraph (b) applies  (b) If no correct child restraint is available in the front or rear —  (i) an adult seat belt must be worn in minibuses  (ii) in light goods vehicles (up to 3500kg max design weight) an adult seat belt may be worn only if the driver can prove that the child, because of an unexpected necessity, is being carried 5 miles or less	Driver
Child at or over 1.35 metres (4ft 5ins) or who is aged 12 or 13 years	Correct child restraint or adult belt must be worn in coaches, goods vehicles and minibuses	Correct child restraint or adult belt must be worn in goods vehicles and minibuses	Driver

## **EXEMPTIONS**

Apart from the persons exempted in the above tables, certain other persons are exempted from wearing a seat belt, for example –

- a person using a motor cycle with or without a sidecar
- a person holding a prescribed medical certificate
- a disabled person using a disabled person's belt
- an adult performing a reversing manœuvre or a qualified driver supervising it
- an adult in a goods vehicle being used for collection or delivery, provided that the journey does not exceed 50 metres
- an adult in a vehicle being used by the emergency services
- a taxi driver
- an adult in a vehicle being tested under a trade licence
- an adult driving or riding in an older passenger car not fitted with seat belts
- a child riding in a vehicle being used by the police or the ambulance service.

[Road Traffic Act 1985, s. 25 (as amended), and Motor Vehicles (Wearing of Seat Belts Regulations 2007]

## 100

Child restraints. It is important to choose the correct child restraint for the age and weight of the child.

### CHILD RESTRAINTS

	<b>Seat facing</b>	<b>Weight and Age</b>	<b>Typical Examples</b>
Baby seat (frontal airbag must be deactivated)	Rear-facing	For children up to 13 kg (approx from birth to 9-12 months)	
Child seat	Forward-facing	For children 9 kg to 18 kg (approx 9 months to 4 years)	
Booster seat		For children 15 kg and up (from approx 4 years)	
Booster cushion		For children from 22 kg (from approx 6 years)	



**Rule 100: Make sure that a child uses a suitable restraint which is correctly adjusted**

## 101

Child restraints. Vehicle manufacturers advise against using rear-facing child restraints with air bags because, in the event of a crash, the air bag will hit the back of a child restraint, pushing it up and back towards the rear of the vehicle and potentially forcing a child out of the restraint. For this reason a rear-facing baby

seat **MUST NOT** be fitted into a seat protected by a frontal air bag unless the air bag has been de-activated.

[Motor Vehicles (Wearing of Seat Belts) Regulations 2007, reg 6(a)]

## 102

Children in cars, vans and other goods vehicles. Drivers who are carrying children in such vehicles should also ensure that —

- children get into the vehicle through the door nearest the kerb
- child restraints are properly fitted to the manufacturer's instructions
- children do not sit behind the rear seats in an estate car or hatchback unless a special child seat has been fitted
- the child safety door locks, where fitted, are used when children are in the vehicle
- children are kept under control.

## **General rules, techniques and advice for all drivers and riders (103-158)**

### **Signals (103-112)**

## 103

Signals warn and inform other road users, including pedestrians, of your intended actions (see Annex 2: "Signals to other road users"). You should always —

- give clear signals in plenty of time, having checked it is not misleading to signal at that time
- use them to advise other road users before changing course or direction, stopping or moving off
- cancel them after use
- make sure your signals will not confuse others. If, for instance, you want to stop after a side road, do not signal until you are passing the road. If you signal earlier it may give the impression that you intend to turn into the road. Your brake lights will warn traffic behind you that you are slowing down
- use an arm signal to emphasise or reinforce your signal if necessary. Remember that signalling does not give you priority.

## 104

You should also —

- watch out for signals given by other road users and proceed only when you are satisfied that it is safe
- be aware that an indicator on another vehicle may not have been cancelled.

## **105**

You **MUST** obey signals given by police officers and traffic wardens (see Annex 5: "Signals by authorised persons") and signs used by school crossing patrols.

[Road Traffic Act 1985, ss. 16, 17 and 40, Functions of Traffic Wardens Order 1989, article 3(2)(a) and (b), and Road Traffic Regulation Act 1985, s. 9]

## **106**

Police stopping procedures. If the police want to stop your vehicle they will, where possible, attract your attention by —

- flashing blue lights, headlights or sounding their siren or horn, usually from behind
- directing you to pull over to the side by pointing and/or using the left indicator.

You **MUST** then pull over and stop as soon as it is safe to do so. Then switch off your engine.

[Road Traffic Act 1985, s. 40]

## **Other stopping procedures**

## **107**

Authorised examiners of the Vehicle and Driving Test Centre. Such examiners have the power to stop vehicles on all roads for the purpose of testing, provided that a police constable is present. They will wear distinguishing clothing.

At least one sign indicating a police presence and warning drivers of oncoming vehicles that they may be required to stop will be displayed so as to be visible to them at such a distance from the area where the test is to be conducted as will enable them to stop safely if required to do so.

[Road Traffic Act 1985, Schedule 2, para 4(1A)]

## **108**

Compliance with directions of authorised examiners. It is an offence not to stop as required by authorised examiners. You **MUST** obey any signals given.

[Road Traffic Act 1985, Schedule 2, para 4(1B)]

## **109**

Traffic light signals and traffic signs. You **MUST** obey all traffic light signals and traffic signs giving orders, including temporary signals and signs (see Annex 6: "Traffic signs... " — "Signs giving orders", "Warning Signs", and "Direction signs"). Make sure you know, understand and act on all other traffic and information signs and road markings (see Annex 6: "Traffic signs... " — "Signs giving orders", "Warning Signs", "Direction signs", and "Information signs"; Annex 8: "Road markings"; and Annex 9: "Vehicle markings").

[Road Traffic Act 1985 s. 16, and Traffic Signs Regulations 2002, regs 10, 15, 16, 25, 26, 27, 28, 29, 36, and 40 as they have effect with modifications in the Isle of Man by virtue of the Traffic Signs (Application) Regulations 2003]

## **110**

Flashing headlights. Only flash your headlights to let other road users know that you are there. Do not flash your headlights to convey any other message or intimidate other road users.

## **111**

Never assume that flashing headlights are a signal inviting you to proceed. Use your own judgement and proceed carefully.

## **112**

The horn. Use only while your vehicle is moving and you need to warn other road users of your presence. Never sound your horn aggressively. You **MUST NOT** use your horn —

- while stationary on a road except at times of danger due to another moving vehicle
- when driving between 11.30 pm and 7.00 am in a built-up area or on a road to which a speed limit applies.

[Road Vehicles (Maintenance and Use) Regulations 2012, reg 20(2)]

## **Lighting requirements (113-116)**

### **113**

You **MUST** —

- ensure all sidelights and rear registration plate lights are lit between sunset and sunrise
- use headlights at night
- use headlights when visibility is seriously reduced (see Rule 226).

Night (the hours of darkness) is defined as the period between half an hour after sunset and half an hour before sunrise.

[Road Vehicles Lighting Regulations 1989 (as modified by the Road Vehicles Lighting (Application) Regulations 1998 (as amended)), regs 3, 24 and 25]

## 114

You **MUST NOT** —

- use any lights in a way which would dazzle or cause discomfort to other road users, including pedestrians, cyclists and horse riders
- use front or rear fog lights unless visibility is seriously reduced. You **MUST** switch them off when visibility improves to avoid dazzling other road users (see Rule 226).

In stationary queues of traffic drivers should apply the parking brake and, once the following traffic has stopped, take their foot off the footbrake to de-activate the vehicle brake lights. This will minimise glare to road users behind until the traffic moves again.

[Road Vehicles Lighting Regulations 1989 (as modified), reg 27]

## 115

You should also —

- use dipped headlights, or dim-dip if fitted, in dull daytime weather to ensure that you can be seen
- keep your headlights dipped when overtaking until you are level with the other vehicle and then change to main beam if necessary, unless this would dazzle oncoming road users
- slow down, and if necessary stop, if you are dazzled by oncoming headlights.

## 116

Hazard warning lights. These may be used when your vehicle is stationary so as to warn that it is temporarily obstructing traffic. Never use them as an excuse for dangerous or illegal parking. You **MUST NOT** use hazard warning lights while driving or being towed.

[Road Vehicles Lighting Regulations 1989 (as modified), reg 27]

# **Control of the vehicle (117-126)**

## **Braking**

**117**

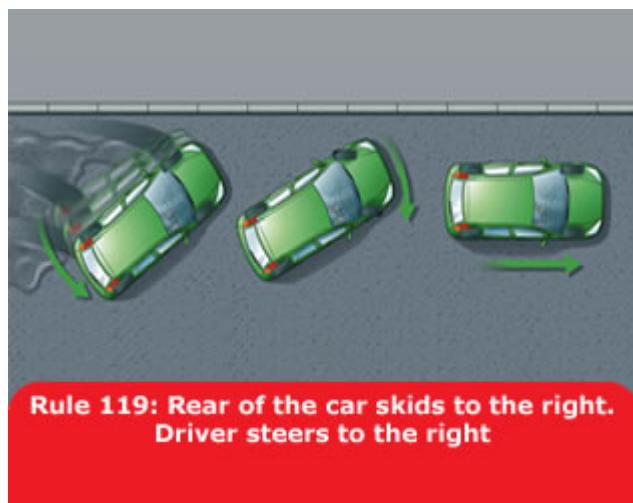
In normal circumstances. The safest way to brake is to do so early and lightly. Brake more firmly as you begin to stop. Ease the pressure off just before the vehicle comes to rest to avoid a jerky stop.

**118**

In an emergency. Brake immediately. Try to avoid braking so harshly that you lock your wheels. Locked wheels can lead to loss of control.

**119**

Skids. Skidding is usually caused by the driver braking, accelerating or steering too harshly or driving too fast for the road conditions. If skidding occurs, remove the cause by releasing the brake pedal fully or easing off the accelerator. Turn the steering wheel in the direction of the skid. For example, if the rear of the vehicle skids to the right, steer immediately to the right to recover.



**120**

ABS. If your vehicle is fitted with anti-lock brakes, you should follow the advice given in the vehicle handbook. However, in the case of an emergency, apply the footbrake firmly; do not release the pressure until the vehicle has slowed to the desired speed. The ABS should ensure that steering control will be retained, but do not assume that a vehicle with ABS will stop in a shorter distance.

## **121**

Brakes affected by water. If you have driven through deep water your brakes may be less effective. Test them at the first safe opportunity by pushing gently on the brake pedal to make sure that they work. If they are not fully effective, gently apply light pressure while driving slowly. This will help to dry them out.

## **122**

Coasting. This term describes a vehicle travelling in neutral or with the clutch pressed down. It can reduce driver control because —

- engine braking is eliminated
- vehicle speed downhill will increase quickly
- increased use of the footbrake can reduce its effectiveness
- steering response will be affected, particularly on bends and corners
- it may be more difficult to select the appropriate gear when needed.

## **123**

The driver and the environment. You **MUST NOT** leave a parked vehicle unattended with the engine running or leave a vehicle engine running unnecessarily while that vehicle is stationary on a public road. Generally, if the vehicle is stationary and is likely to remain so for more than a couple of minutes, you should apply the parking brake and switch off the engine to reduce emissions and noise pollution. However it is permissible to leave the engine running if the vehicle is stationary in traffic or for diagnosing faults.

[Road Vehicles (Maintenance and Use) Regulations 2012, regs 18 and 64]

## **Speed limits**

### **124**

You **MUST NOT** exceed the maximum speed limits for the road and for your vehicle.

#### **ROADS IN TOWN OR VILLAGE DISTRICTS**

A speed limit of 30 mph applies to all traffic on such roads unless a lower speed limit, say 20 mph, applies to —

- certain residential roads, or
- motor vehicles of a certain class (see below).

#### **SPEED LIMITS FOR MOTOR VEHICLES OF CERTAIN CLASSES**

The table below specifies the speed limits for certain classes of motor vehicles used on unrestricted roads or unrestricted parts of roads. If such limits are below 30

mph, they also apply when such vehicles are used on roads in town or village districts. An unrestricted road (or part) is a road (or part) that lies outside such a district and is subject to no speed limit.

<b><i>Item.</i></b>	<b><i>Class of motor vehicles</i></b>	<b><i>Speed limit (mph)</i></b>
1.	A motor cycle drawing a trailer	40
2.	A passenger car or dual-purpose vehicle —	
	(a) drawing a trailer	40
	(b) drawing more than one trailer	20
3.	An invalid carriage	20
4.	A motor caravan not exceeding a maximum design weight of 3500 kg —	
	(a) not drawing a trailer	60
	(b) drawing a trailer	40
	(c) drawing more than one trailer	20
5.	A motor caravan exceeding a maximum design weight of 3500 kg —	
	(a) not drawing a trailer	50
	(b) drawing a trailer	40
	(c) drawing more than one trailer	20
6.	A minibus —	
	(a) not drawing a trailer	50
	(b) drawing a trailer	40
	(c) drawing more than one trailer	20
7.	A bus constructed or adapted to carry more than 16 seated passengers in addition to the driver —	
	(a) not drawing a trailer	40
	(b) drawing a trailer	40
	(c) drawing more than one trailer	20
8.	A goods vehicle (including a car-derived van) not exceeding a maximum design weight of 3500 kg —	
	(a) not drawing a trailer	60
	(b) drawing a trailer	40
	(c) drawing more than one trailer	20

9.	A goods vehicle or fire tender (neither of which is an articulated vehicle) exceeding a maximum design weight of 3500 kg but not exceeding a maximum design weight of 7500 kg —	
	(a) not drawing a trailer	50
	(b) drawing a trailer	40
	(c) drawing more than one trailer	20
10.	A goods vehicle or fire tender (neither of which is an articulated vehicle) exceeding a maximum design weight of 7500 kg —	
	(a) not drawing a trailer	40
	(b) drawing a trailer	40
	(c) drawing more than one trailer	20
11.	An articulated vehicle	40*
12.	Agricultural motor vehicle —	
	(a) not drawing a trailer	40
	(b) drawing a trailer	30
	(c) drawing more than one trailer	20

\*If —

- (a) an articulated vehicle exceeds 32520 kg but not 38000 kg, and
- (b) every driving axle of the motor vehicle and every axle of the trailer is not fitted with road-friendly suspension,

then the articulated vehicle must not be driven at a speed exceeding 30 mph.

[Road Traffic Regulation Act 1985, ss. 23(1)(a) and 25(1), Motor Vehicles (Speed Limits) Regulations 1992 as amended by the Motor Vehicles (Speed Limits) (Amendment) Regulations 2011, and Road Vehicles (Maintenance and Use) Regulations 2012, reg 79]

## 125

The speed limit is the absolute maximum and does not mean it is safe to drive at that speed irrespective of conditions. Driving at speeds too fast for the road and traffic conditions is dangerous. You should always reduce your speed when —

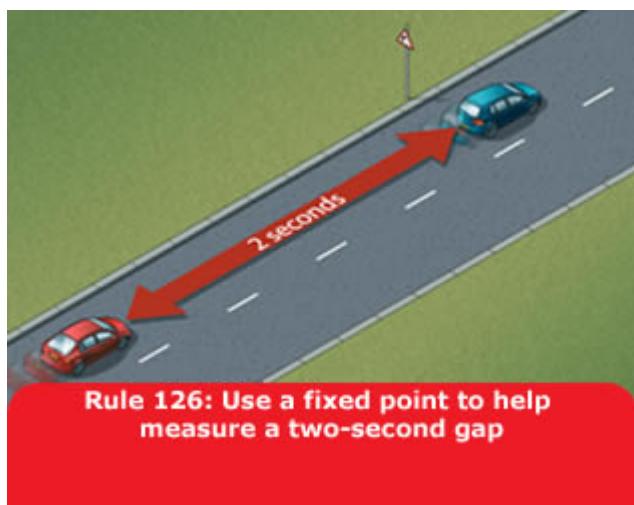
- the road layout or condition presents hazards such as bends
- sharing the road with motorcyclists or with pedestrians, pedal cyclists and horse riders, particularly children
- weather conditions make it safer to do so
- driving at night as it is more difficult to see other road users.

## Stopping Distances

**126**

Drive at a speed that will allow you to stop well within the distance you can see to be clear. You should —

- leave enough space between you and the vehicle in front so that you can pull up safely if it suddenly slows down or stops. The safe rule is never to get closer than the overall stopping distance (see Annex 7: "Typical Stopping Distances")
- allow at least a two-second gap between you and the vehicle in front on roads carrying faster-moving traffic and in tunnels where visibility is reduced. The gap should be at least doubled on wet roads and increased still further on icy roads
- remember that large vehicles and motorcycles need a greater distance to stop. If driving a large vehicle in a tunnel, you should allow a four-second gap between you and the vehicle in front
- if you have to stop in a tunnel, leave at least a 5-metre gap between you and the vehicle in front.



## Lines and lane markings on the road (127-132)

See Annex 8: "Road markings" for diagrams of all lines.

**127**

A broken white line. This marks the centre of the road. When this line lengthens and the gaps shorten, it means that there is a hazard ahead. Do not cross it unless you can see the road is clear and wish to overtake or turn off.

## **128**

Double white lines where the line nearest to you is broken. This means you may cross the lines to overtake if it is safe, provided you can complete the manoeuvre before reaching a solid white line on your side. White direction arrows on the road indicate that you need to get back on to your side of the road.

## **129**

Double white lines where the line nearest you is solid. This means you **MUST NOT** cross or straddle it unless it is safe and you need to enter adjoining premises or a side road. You may cross the line if necessary, provided the road is clear, to pass a stationary vehicle or to overtake a pedal cycle, horse or road maintenance vehicle if they are travelling at 10 mph or less.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10 and 26 as modified by the Traffic Signs (Application) Regulations 2003]

## **130**

Areas of white diagonal stripes or chevrons painted on the road. These are to separate traffic lanes or to protect traffic turning right.

- If the area is bordered by a broken white line, you should not enter the area unless it is necessary and you can see that it is safe to do so.
- If the area is bordered by solid white lines you **MUST NOT** enter it except in an emergency.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, reg 10 (as modified)]

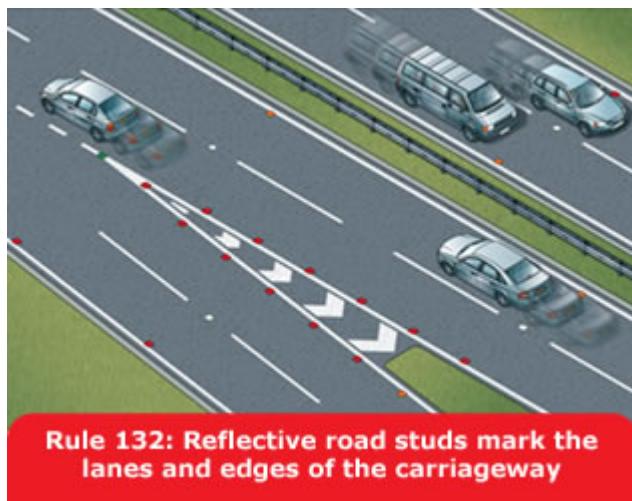
## **131**

Lane dividers. These are short, broken white lines which are used on wide carriageways to divide them into lanes. You should keep between them.

## **132**

Reflective road studs may be used with white lines —

- white studs mark the lanes or the middle of the road
- red studs mark the left edge of the road
- amber studs mark the central reservation of a dual carriageway or motorway in Great Britain
- green studs mark the edge of the main carriageway at lay-bys and slip roads
- green/yellow studs indicate temporary adjustments to lane layouts, e.g. where road works are taking place.



## **Multi-lane carriageways (133-143)**

### **Lane discipline**

#### **133**

If you need to change lane, first use your mirrors and if necessary take a quick sideways glance to make sure you will not force another road user to change course or speed. When it is safe to do so, signal to indicate your intentions to other road users and, when clear, move over.

#### **134**

You should follow the signs and road markings and get into the lane as directed. In congested road conditions do not change lanes unnecessarily. Merging in turn is recommended but only if safe and appropriate when vehicles are travelling at a very low speed, e.g. when approaching road works or a road traffic incident. It is not recommended at high speed.

### **Single carriageway**

#### **135**

Where a single carriageway in Great Britain has three lanes and the road markings or signs do not give priority to traffic in either direction —

- use the middle lane only for overtaking or turning right. Remember, you have no more right to use the middle lane than a driver coming from the opposite direction
- do not use the right-hand lane.

## **136**

Where a single carriageway has four or more lanes, use only the lanes that signs or markings indicate.

### **Dual carriageways, preponderantly in Great Britain**

A dual carriageway is a road which has a central reservation to separate the carriageways.

## **137**

On a two-lane dual carriageway you should, unless traffic signs or road markings indicate otherwise, stay in the left-hand lane and use the right-hand lane for overtaking or turning right. If you are permitted to use the right-hand lane for overtaking, move back to the left-hand lane when it is safe to do so.

## **138**

On a three-lane dual carriageway, you may use the middle lane or the right-hand lane to overtake but return to the middle and then the left-hand lane when it is safe.

### **Lanes for the use of traffic in Great Britain**

## **139**

Climbing and crawler lanes. These are provided on some hills. Use this lane if you are driving a slow-moving vehicle or if there are vehicles behind you wishing to overtake. Be aware of the signs and road markings which indicate the lane is about to end.

## **140**

Cycle lanes. These are shown by road markings and signs. You **MUST NOT** drive or park in a cycle lane marked by a solid white line during its times of operation. Do not drive or park in a cycle lane marked by a broken white line unless it is unavoidable. You **MUST NOT** park in any cycle lane whilst waiting restrictions apply.

[GB Road Traffic Regulation Act 1984, ss. 5 and 8]

## **141**

Bus lanes. These are shown by road markings and signs that indicate which (if any) other vehicles are permitted to use the bus lane. Unless otherwise indicated, you should not drive in a bus lane during its period of operation. You may enter a bus lane to stop, to load or unload where this is not prohibited.

## 142

High-occupancy vehicle lanes and other designated vehicle lanes. Lanes may be restricted for use by particular types of vehicle, and such restrictions may apply some or all of the time. The operating times and vehicle types will be indicated on the accompanying traffic signs. You **MUST NOT** drive in such lanes during their times of operation unless signs indicate that your vehicle is permitted (see Annex 6: "Traffic signs..." — "Information signs").

Vehicles permitted to use designated lanes may or may not include pedal cycles, buses, taxis, licensed private-hire vehicles, motorcycles, heavy goods vehicles and high-occupancy vehicles (HOVs).

Where HOV lanes are in operation, they **MUST ONLY** be used by —

- vehicles containing at least the minimum number of people indicated on the traffic signs
- any other vehicles, such as buses and motorcycles, as indicated on signs prior to the start of the lane, irrespective of the number of occupants.

[GB Road Traffic Regulation Act 1984, ss. 5 and 8, and GB Road Traffic Act 1985, s. 36]

## One-way streets

## 143

One-way streets. Traffic **MUST** travel in the direction indicated by signs. Buses and/or cycles in Great Britain may have a contra-flow lane. Choose the correct lane for your exit as soon as you can. Do not change lanes suddenly. Unless road signs or markings indicate otherwise, you should use —

- the left-hand lane when going left
- the right-hand lane when going right
- the most appropriate lane when going straight ahead. Remember — traffic could be passing on both sides.

[Road Traffic Act 1985, s. 16]

## General advice (144-158)

## 144

You **MUST NOT** —

- drive dangerously
- drive without due care and attention

- drive without reasonable consideration for other road users.

[Road Traffic Act 1985, ss. 2 and 3]

## 145

You **MUST NOT** drive without lawful authority on or over a pavement, footpath, cycle-path, bridle-path or any other land not forming part of a road.

[Road Traffic Act 1985, ss. 28 and 30]

## 146

Adapt your driving to the appropriate type and condition of road you are on. In particular —

- do not treat speed limits as a target. It is often not appropriate or safe to drive at the maximum speed limit
- take the road and traffic conditions into account. Be prepared for unexpected or difficult situations, for example the road being blocked beyond a blind bend. Be prepared to adjust your speed as a precaution
- where there are junctions, be prepared for road users emerging
- in side roads and country lanes look out for unmarked junctions where nobody has priority
- be prepared to stop at traffic control systems, road works, pedestrian crossings or traffic lights as necessary
- try to anticipate what pedestrians and cyclists might do. If pedestrians, particularly children, are looking the other way, they may step out into the road without seeing you.

## 147

Be considerate. Be careful of and considerate towards all types of road users, especially those requiring extra care (see Rule 204). You should —

- try to be understanding if other road users cause problems; they may be inexperienced or not know the area well
- be patient; remember that anyone can make a mistake
- not allow yourself to become agitated or involved if someone is behaving badly on the road. This will only make the situation worse. Pull over, calm down and, when you feel relaxed, continue your journey
- slow down and hold back if a road user pulls out into your path at a junction. Allow them to get clear. Do not over-react by driving too close behind to intimidate them
- not throw anything out of a vehicle, for example cigarette ends, cans, paper or carrier bags. This can endanger other road users, particularly motorcyclists and pedal cyclists.

## **148**

Safe driving and riding needs concentration.

Avoid distractions when driving or riding such as —

- loud music (this may mask other sounds)
- trying to read maps
- inserting a cassette or CD or tuning a radio
- arguing with your passengers or other road users
- eating and drinking
- smoking.

## **Mobile phones and in-vehicle technology**

## **149**

You **MUST** exercise proper control of your vehicle at all times. You **MUST NOT** use a hand-held mobile phone or similar device when driving and should not do so when supervising a learner driver. Never use a hand-held microphone when driving. Using hands-free equipment is also likely to distract your attention from the road. It is far safer not to use any telephone while you are driving or riding — find a safe place to stop first or use the voicemail facility and listen to messages later.

[Road Traffic Act 1985, ss. 2 and 3, and Road Vehicles (Maintenance and Use) Regulations 2012, reg 68]

## **150**

There is a danger of driver distraction being caused by in-vehicle systems such as satellite navigation systems, congestion warning systems, PCs, multi-media, etc. You **MUST** exercise proper control of your vehicle at all times. Do not rely on driver assistance systems such as cruise control or lane departure warnings. They are available to assist but you should not reduce your concentration levels. Do not be distracted by maps or screen-based information (such as navigation or vehicle management systems) while driving or riding. If necessary find a safe place to stop.

[Road Traffic Act 1985, ss. 2 and 3, and Road Vehicles (Maintenance and Use) Regulations 2012, reg 61]

## **151**

In slow-moving traffic. You should —

- reduce the distance between you and the vehicle ahead to maintain traffic flow
- never get so close to the vehicle in front that you cannot stop safely

- leave enough space to be able to manoeuvre if the vehicle in front breaks down or an emergency vehicle needs to get past
- not change lanes to the left to overtake
- allow access into and from side roads as blocking these will add to congestion
- be aware of pedal cyclists and motorcyclists who may be passing on either side.



## Driving in built-up areas

### 152

Residential streets. You should drive slowly and carefully on streets where there are likely to be pedestrians, cyclists and parked cars. In some areas a 20 mph maximum speed limit may be in force. Look out for —

- vehicles emerging from junctions or driveways
- vehicles moving off
- car doors opening
- pedestrians
- children running out from between parked cars
- pedal cyclists and motorcyclists.

### 153

Traffic-calming measures. On some roads there are features such as road humps, chicanes and narrowings which are intended to slow you down. When you approach these features reduce your speed. Allow pedal cyclists and motorcyclists room to pass through them. Maintain a reduced speed along the whole of the stretch of road within the calming measures. Give way to oncoming road users if directed to do so by signs. You should not overtake other moving road users while in these areas.



## Country roads

### 154

Take extra care on country roads and reduce your speed at approaches to bends, which can be sharper than they appear, and at junctions and turnings, which may be partially hidden. Be prepared for pedestrians, horse riders, pedal cyclists, slow-moving farm vehicles or mud on the road surface. Make sure you can stop within the distance you can see to be clear. You should also reduce your speed where country roads enter villages. For advice on greenlanes and greenway roads, see Annex 14.

### 155

Single-track roads. These are only wide enough for one vehicle. They may have special passing places. If you see a vehicle coming towards you, or the driver behind wants to overtake, pull into a passing place on your left or wait opposite a passing place on your right. Give way to vehicles coming uphill whenever you can. If necessary, reverse until you reach a passing place to let the other vehicle pass. Slow down when passing pedestrians, pedal cyclists and horse riders.

### 156

Do not park in passing places.

## Vehicles prohibited from using roads and pavements

### 157

Certain motorised vehicles do not meet the construction and technical requirements for road vehicles and are generally neither intended nor suitable or legal for use on roads, pavements, footpaths, cycle-paths or bridle-paths. These include most types of miniature motorcycles, also called mini motos, and motorised scooters, also called go-peds, which are powered by electric or internal combustion engines. These types

of vehicle **MUST NOT** be used on roads, pavements, footpaths, cycle-paths or bridle-paths.

[Road Vehicles (Maintenance and Use) Regulations 2012, Road Vehicles (Construction, Equipment and Use) Regulations 2012, and Road Traffic Act 1985, ss. 28, 30 and Schedule 2, para 1(5)]

## **158**

Certain models of motorcycles, motor tricycles and quadricycles, also called quad bikes, are suitable only for off-road use and do not meet legal standards for use on roads. Vehicles that do not meet these standards **MUST NOT** be used on roads. **NOR MUST** they be used on pavements, footpaths, cycle-paths or bridle-paths. You **MUST** make sure that any motorcycle, motor tricycle, quadricycle or any other motor vehicle meets legal standards and is properly registered, taxed and insured before using it on roads. Even when registered, taxed and insured for the road, vehicles **MUST NOT** be used on pavements, footpaths, cycle-paths or bridle-paths.

[Road Vehicles (Maintenance and Use) Regulations 2012, Road Vehicles (Construction, Equipment and Weights) Regulations 2012, Road Traffic Act 1985, ss. 28, 30, Schedule 2, para 1(5), and Schedule 5, para 1(1), and Licensing and Registration of Vehicles Act 1985, ss. 1, 5, 11, and 12]

## **Using the road (159-203)**

### **General rules (159-161)**

#### **159**

Before moving off you should —

- use all mirrors to check the road is clear
- look round to check the blind spots (the areas you are unable to see in the mirrors)
- signal if necessary before moving out
- look round for a final check.

Move off only when it is safe to do so.



**Rule 159: Check the blind spot before moving off**

## 160

Once moving you should —

- keep to the left unless road signs or markings indicate otherwise. The exceptions are when you want to overtake, turn right or pass parked vehicles or pedestrians in the road
- keep well to the left on right-hand bends. This will improve your view of the road and help avoid the risk of colliding with traffic approaching from the opposite direction
- drive with both hands on the wheel where possible. This will help you to remain in full control of the vehicle at all times
- be aware of other road users, especially pedal cycles and motorcycles who may be filtering through the traffic. These are more difficult to see than larger vehicles and their riders are particularly vulnerable. Give them plenty of room, especially if you are driving a long vehicle or towing a trailer
- select a lower gear before you reach a long downhill slope. This will help to control your speed
- when towing, remember the extra length will affect overtaking and manoeuvring. The extra weight will also affect the braking and acceleration.

## 161

Mirrors. All mirrors should be used effectively throughout your journey. You should —

- use your mirrors frequently so that you always know what is behind and to each side of you
- use them in good time before you signal or change direction or speed
- be aware that mirrors do not cover all areas and there will be blind spots. You will need to look round and check.

Remember: Mirrors — Signal — Manoeuvre.

## Overtaking (162-169)

### 162

Before overtaking you should make sure —

- the road is sufficiently clear ahead
- road users are not beginning to overtake you
- there is a suitable gap in front of the road user you plan to overtake.

### 163

Overtake only when it is safe and legal to do so. You should —

- not get too close to the vehicle you intend to overtake
- use your mirrors, signal when it is safe to do so, take a quick sideways glance if necessary into the blind spot area and then start to move out
- not assume that you can simply follow a vehicle ahead which is overtaking — there may only be enough room for one vehicle
- move quickly past the vehicle you are overtaking, once you have started to overtake. Allow plenty of room. Move back to the left as soon as you can but do not cut in
- take extra care at night and in poor visibility when it is harder to judge speed and distance
- give way to oncoming vehicles before passing parked vehicles or other obstructions on your side of the road
- only overtake on the left if the vehicle in front is signalling to turn right, and there is room to do so
- stay in your lane if traffic is moving slowly in queues. If the queue on your right is moving more slowly than you are, you may pass on the left
- give motorcyclists, pedal cyclists and horse riders at least as much room as you would when overtaking a car (see Rules 211-215).



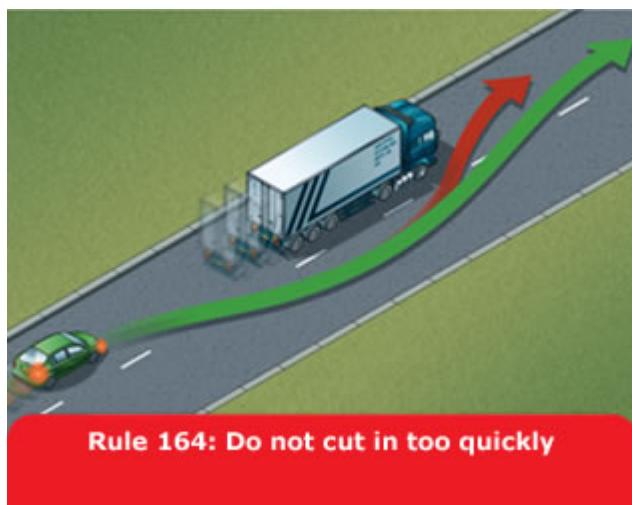
**Rule 163: Give vulnerable road users at least as much space as you would a car**

Remember: Mirrors — Signal — Manoeuvre.

## 164

Large vehicles. Overtaking these is more difficult. You should —

- drop back. This will increase your ability to see ahead and should allow the driver of the large vehicle to see you in his or her mirrors. Getting too close to large vehicles, including agricultural vehicles such as a tractor with a trailer or other fixed equipment, will obscure your view of the road ahead and there may be another slow-moving vehicle in front
- make sure that you have enough room to complete your overtaking manoeuvre before committing yourself. It takes longer to pass a large vehicle. If in doubt do not overtake
- not assume you can follow a vehicle ahead which is overtaking a long vehicle. If a problem develops, they may abort overtaking and pull back in.



## 165

You **MUST NOT** overtake —

- if you would have to cross or straddle double white lines with a solid line nearest to you (but see Rule 129)
- if you would have to enter an area designed to divide traffic when it is surrounded by a solid white line
- the nearest vehicle to a pedestrian crossing, especially when it has stopped to let pedestrians cross
- after a "No Overtaking" sign and until you pass a sign cancelling the restriction.

[Road Traffic Act 1985, s. 16, Traffic Signs Regulations 2002, reg 10, Zebra Pedestrian Crossings Regulations 1972, "Pelican" Pedestrian Crossings Regulations 1990, and Puffin Pedestrian Crossings Regulations 2000]

## **166**

**DO NOT** overtake if there is any doubt, or where you cannot see far enough ahead to be sure it is safe, for example when you are approaching —

- a corner or bend
- a hump bridge
- the brow of a hill.

## **167**

**DO NOT** overtake where you might come into conflict with other road users, for example —

- if they are approaching or at a road junction on either side of the road
- where the road narrows
- when approaching a school crossing patrol
- between the kerb and a bus or tram when it is at a stop
- where traffic is queuing at junctions or road works
- when you would force another road user to swerve or slow down
- at a level crossing
- when a road user is indicating right, even if you believe the signal should have been cancelled. Do not take a risk — wait for the signal to be cancelled
- stay behind if you are following a cyclist approaching a roundabout or junction and you intend to turn left.

## **168**

Being overtaken. If a driver is trying to overtake you, maintain a steady course and speed, slowing down if necessary to let the vehicle pass. Never obstruct drivers who wish to pass. Speeding up or driving unpredictably while someone is overtaking you is dangerous. Drop back to maintain a two-second gap if someone overtakes and pulls into the gap in front of you.

## **169**

Do not hold up a long queue of traffic, especially if you are driving a large or slow-moving vehicle. Check your mirrors frequently and, if necessary, pull in where it is safe and let traffic pass

# **Road junctions (170-183)**

## **170**

Take extra care at junctions. You should —

- watch out for pedal cyclists, motorcyclists, powered wheelchairs/mobility scooters and pedestrians as they are not always easy to see. Be aware that they may not have seen or heard you if you are approaching from behind
- watch out for pedestrians crossing a road into which you are turning. If they have started to cross they have priority, so give way
- watch out for long vehicles which may be turning at a junction ahead. They may have to use the whole width of the road to make the turn (see Rule 221)
- watch out for horse riders who may take a different line on the road from that which you would expect
- not assume, when waiting at a junction, that a vehicle coming from the right and signalling left will actually turn. Wait and make sure
- look all around before emerging. Do not cross or join a road until there is a gap large enough for you to do so safely.



## 171

You **MUST** stop behind the line at a junction with a 'Stop' sign and a solid white line across the road. Wait for a safe gap in the traffic before you move off.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10 and 16]

## 172

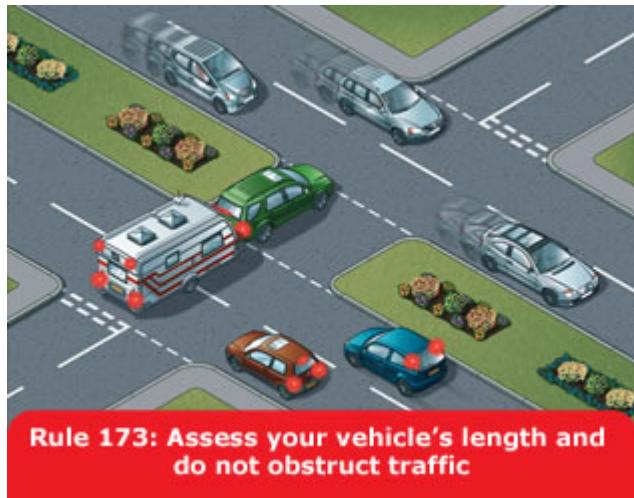
The approach to a junction may have a "Give Way" sign or a triangle marked on the road. You **MUST** give way to traffic on the main road when emerging from a junction with broken white lines across the road.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10, 16 & 25]

## 173

Dual carriageways in Great Britain. When crossing or turning right, first assess whether the central reservation is deep enough to protect the full length of your vehicle.

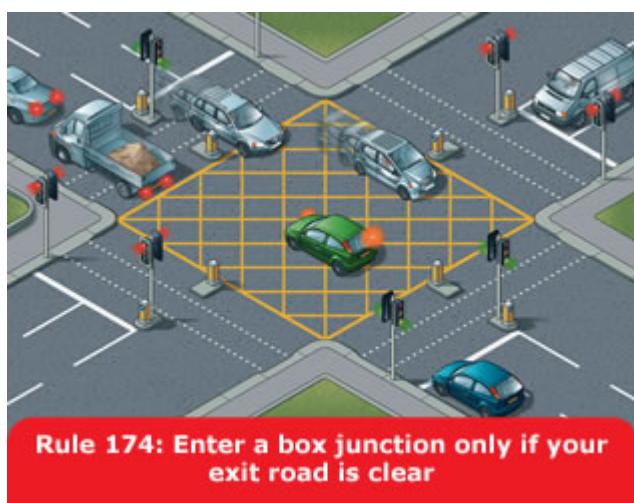
- If it is, then you should treat each half of the carriageway as a separate road. Wait in the central reservation until there is a safe gap in the traffic on the second half of the road.
- If the central reservation is too shallow for the length of your vehicle, wait until you can cross both carriageways in one go.



## 174

Box junctions. These have criss-cross yellow lines painted on the road (see Annex 8: "Road markings"). You **MUST NOT** enter the box until your exit road or lane is clear. However, you may enter the box and wait when you want to turn right and are only stopped from doing so by oncoming traffic or by other vehicles waiting to turn right. In Great Britain you **MUST NOT** enter the box at signalled roundabouts unless you can cross over it completely without stopping.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10 and 29(2)]



## Junctions controlled by traffic lights

**175**

You **MUST** stop behind the white "Stop" line across your side of the road unless the light is green. If the amber light appears you may go on only if you have already crossed the stop line or are so close to it that to stop might cause a collision.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10 and 36]]

**176**

You **MUST NOT** move forward over the white line when the red light is showing. Only go forward when the traffic lights are green if there is room for you to clear the junction safely or you are taking up a position to turn right. If the traffic lights are not working, treat the situation as you would an unmarked junction and proceed with great care.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10 and 36]]

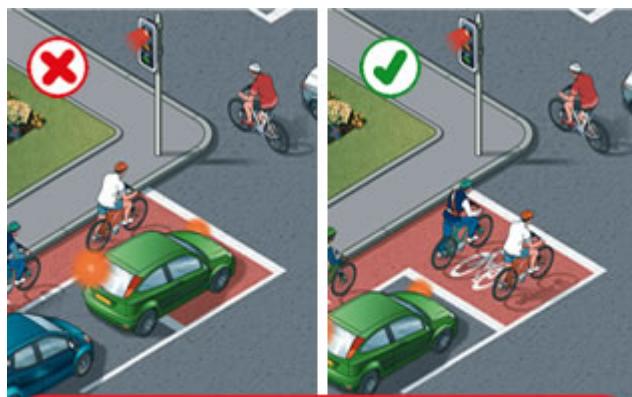
**177**

Green filter arrow. This indicates a filter lane only. Do not enter that lane unless you want to go in the direction of the arrow. Give other traffic, especially pedal cyclists, time and room to move into the correct lane. You may proceed in the direction of the green arrow when it shows. In certain locations the green filter arrow may be preceded by an amber filter arrow, which, when shown, indicates that the green filter arrow is about to show.

**178**

Advanced stop lines in Great Britain. Some signal-controlled junctions there have advanced stop lines to allow pedal cycles to be positioned ahead of other traffic. Motorists, including motorcyclists, **MUST** stop at the first white line reached if the lights are amber or red and should avoid blocking the way or encroaching on the marked area at other times, e.g. if the junction ahead is blocked. If your vehicle has proceeded over the first white line at the time that the signal goes red, you **MUST** stop at the second white line, even if your vehicle is in the marked area. Allow pedal cyclists time and space to move off when the green signal shows.

[GB Road Traffic Act 1988, s. 36, and Traffic Signs Regulations 2002, regs 10, 36(1) and 43(2)]



**Rule 178: Do not unnecessarily encroach on the cyclists' waiting area**

## Turning right

### 179

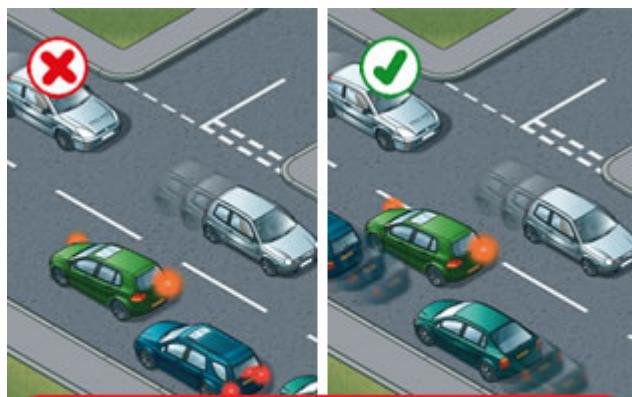
Well before you turn right you should —

- use your mirrors to make sure you know the position and movement of traffic behind you
- give a right-turn signal
- take up a position just left of the middle of the road or in the space marked for traffic turning right
- leave room for other vehicles to pass on the left, if possible.

### 180

Wait until there is a safe gap between you and any oncoming vehicle. Watch out for pedal cyclists, motorcyclists, pedestrians and other road users. Check your mirrors and blind spot again to make sure you are not being overtaken, and then make the turn. Do not cut the corner. Take great care when turning into a main road; you will need to watch for traffic in both directions and wait for a safe gap.

Remember: Mirrors — Signal — Manoeuvre.

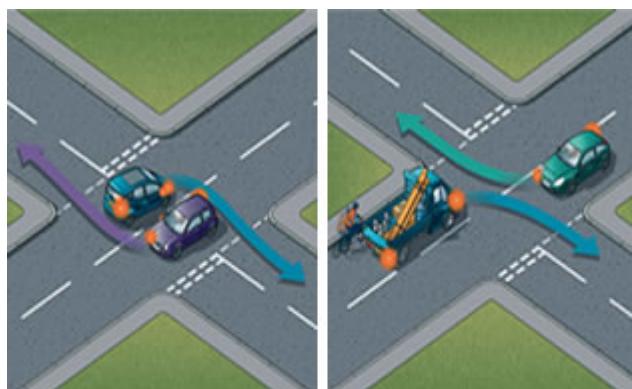


**Rule 180: Position your vehicle correctly to avoid obstructing traffic**

## 181

When turning right at crossroads where an oncoming vehicle is also turning right, there is a choice of two methods —

- turn right side to right side; keep the other vehicle on your right and turn behind it. This is generally the safer method as you have a clear view of any approaching traffic when completing your turn
- left side to left side, turning in front of each other. This can block your view of oncoming vehicles, so take extra care. Pedal cyclists and motorcyclists in particular may be hidden from your view. Road layout, markings or how the other vehicle is positioned can determine which course should be taken.



**Rule 181: Left - Turning right side to right side. Right - Turning left side to left side**

## Turning left

## 182

Use your mirrors and give a left-turn signal well before you turn left. Do not overtake just before you turn left and watch out for traffic coming up on your left

before you make the turn, especially if driving a large vehicle. Pedal cyclists, motorcyclists and other road users in particular may be hidden from your view.



## 183

When turning —

- keep as close to the left as is safe and practicable
- give way in Great Britain to any vehicles using a bus lane, cycle lane or tramway from either direction.

## Roundabouts (184-190)

### 184

On approaching a roundabout take notice and act on all the information available to you, including traffic signs, traffic lights and lane markings which direct you into the correct lane. You should —

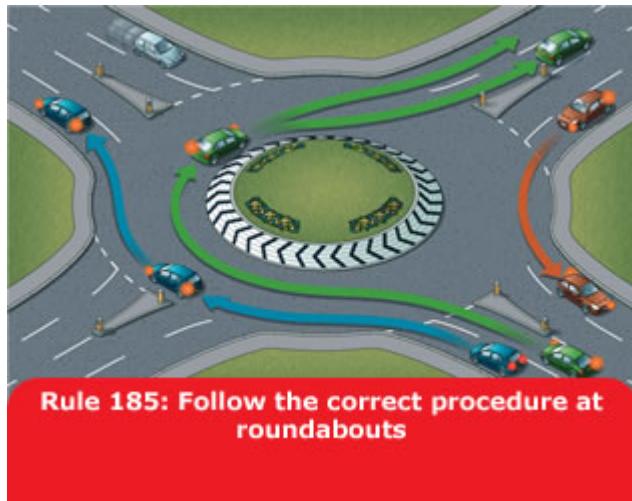
- use "Mirrors — Signal — Manoeuvre" at all stages
- decide as early as possible which exit you need to take
- give an appropriate signal (see Rule 186). Time your signals so as not to confuse other road users
- get into the correct lane
- adjust your speed and position to fit in with traffic conditions
- be aware of the speed and position of all the road users around you.

### 185

When reaching the roundabout you should —

- give priority to traffic on the roundabout approaching from your right, unless directed otherwise by signs, road markings or traffic lights

- check in Great Britain whether road markings allow you to enter the roundabout without giving way. If so, proceed, but still look to the right before joining
- watch out for all other road users already on the roundabout. Be aware they may not be signalling correctly or at all
- look forward before moving off to make sure traffic in front has moved off.



## 186

Signals and position.

When taking the first exit to the left, unless signs or markings indicate otherwise, —

- signal left and approach in the left-hand lane
- keep to the left on the roundabout and continue signalling left to leave.

When taking an exit to the right or going full circle, unless signs or markings indicate otherwise, —

- signal right and approach in the right-hand lane
- keep to the right on the roundabout until you need to change lanes to exit the roundabout
- signal left after you have passed the exit before the one you want.

When taking any intermediate exit, unless signs or markings indicate otherwise, —

- select the appropriate lane on approach to the roundabout
- you should not normally need to signal on approach
- stay in this lane until you need to alter course to exit the roundabout
- signal left after you have passed the exit before the one you want.

When in Great Britain there are more than three lanes at the entrance to a roundabout, use the most appropriate lane on approach and through it.

## **187**

In all cases watch out for and give plenty of room to —

- pedestrians who may be crossing the approach and exit roads
- traffic crossing in front of you on the roundabout, especially vehicles intending to leave by the next exit
- traffic which may be straddling lanes or positioned incorrectly
- motorcyclists
- pedal cyclists and horse riders who may stay in the left-hand lane and signal right if they intend to continue round the roundabout. Allow them to do so
- long vehicles (including those towing trailers). These might have to take a different course or straddle lanes either approaching or on the roundabout because of their length. Watch out for their signals.

## **188**

Mini-roundabouts. Approach these in the same way as normal roundabouts. All vehicles **MUST** pass round the central markings except large vehicles which are physically incapable of doing so. Remember, there is less space to manoeuvre and less time to signal. Avoid making U-turns at mini-roundabouts. Beware of others doing this.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10 and 16]

## **189**

At double mini-roundabouts in Great Britain treat each roundabout separately and give way to traffic from the right.

## **190**

Multiple roundabouts in Great Britain. At some complex junctions, there may be a series of mini-roundabouts at each intersection. Treat each mini-roundabout separately and follow the normal rules.



**Rule 190: Treat each roundabout separately**

## Pedestrian crossings (191-199)

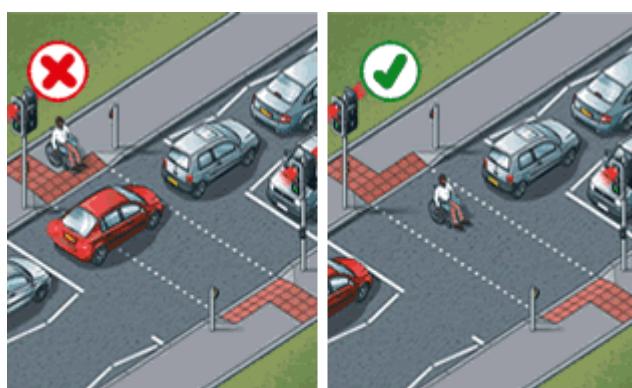
### 191

You **MUST NOT** park on a crossing or in the area covered by the zig-zag lines. You **MUST NOT** overtake the moving vehicle nearest the crossing or the vehicle nearest the crossing which has stopped to give way to pedestrians.

[Road Traffic Act 1985, s. 16, Road Traffic Regulation Act 1985, s. 7(4), Traffic Signs Regulations 2002, regs 10, 27 and 28, Zebra Pedestrian Crossings Regulations 1972, "Pelican" Pedestrian Crossings Regulations 1990, and Puffin Pedestrian Crossings Regulations 2000]

### 192

In queuing traffic, you should keep the crossing clear.



**Rule 192: Keep the crossing clear**

## 193

You should take extra care where the view of either side of the crossing is blocked by queuing traffic or incorrectly parked vehicles. Pedestrians may be crossing between stationary vehicles.

## 194

Allow pedestrians plenty of time to cross and do not harass them by revving your engine or edging forward.

## 195

Zebra crossings. As you approach a zebra crossing —

- look out for pedestrians waiting to cross and be ready to slow down or stop to let them cross
- you **MUST** give way when a pedestrian has moved on to a crossing
- allow more time for stopping on wet or icy roads
- do not wave or use your horn to invite pedestrians across; this could be dangerous if another vehicle is approaching
- be aware of pedestrians approaching from the side of the crossing

A zebra crossing with a central island is two separate crossings (see Rule 20).



[Road Traffic Regulation Act 1985, s. 7(4), and Zebra Pedestrian Crossings Regulations 1972]

## Signal-controlled crossings

## 196

Pelican crossings. These are signal-controlled crossings where flashing amber follows the red "Stop" light. You **MUST** stop when the red light shows. When the amber

light is flashing, you **MUST** give way to any pedestrians on the crossing. If the amber light is flashing and there are no pedestrians on the crossing, you may proceed with caution.

[Road Traffic Regulation Act 1985, s. 7(4), and "Pelican" Pedestrian Crossings Regulations 1990]



## 197

Pelican crossings which go straight across the road are one crossing, even when there is a central island. You **MUST** wait for pedestrians who are crossing from the other side of the island.

[Road Traffic Regulation Act 1985, s. 7(4), and "Pelican" Pedestrian Crossings Regulations 1990]

## 198

Give way to anyone still crossing after the signal for vehicles has changed to green. This advice applies to all crossings.

## 199

Puffin and toucan crossings. Equestrian crossings in Great Britain. These are similar to pelican crossings, but there is no flashing amber phase; the light sequence for traffic at these three crossings is the same as at traffic lights. If the signal-controlled crossing is not working, proceed with extreme caution.

[Road Traffic Regulation Act 1985, s. 7(4), and Puffin Pedestrian Crossings Regulations 1990]

# Reversing (200-203)

## 200

Choose an appropriate place to manoeuvre. If you need to turn your vehicle around, wait until you find a safe place. Try not to reverse or turn round in a busy road; find a quiet side road or drive round a block of side streets.

## 201

Do not reverse from a side road into a main road. When using a driveway, reverse in and drive out if you can.

## 202

Look carefully before you start reversing. You should —

- use all your mirrors
- check the 'blind spot' behind you (the part of the road you cannot see easily in the mirrors)
- check there are no pedestrians (particularly children), cyclists, other road users or obstructions in the road behind you.

Reverse slowly while —

- checking all around
- looking mainly through the rear window
- being aware that the front of your vehicle will swing out as you turn.

Get someone to guide you if you cannot see clearly.



## 203

You **MUST NOT** reverse your vehicle further than necessary.

## Road users requiring extra care (204-225)

### 204

The most vulnerable road users are pedestrians, pedal cyclists, motorcyclists and horse riders. It is particularly important to be aware of children, older and disabled people, and learner and inexperienced drivers and riders.

#### Pedestrians

### 205

There is a risk of pedestrians, especially children, stepping unexpectedly into the road. You should drive with the safety of children in mind at a speed suitable for the conditions.

### 206

Drive carefully and slowly when —

- in crowded shopping streets, Home Zones or residential areas
- driving past bus and tram stops; pedestrians may emerge suddenly into the road
- passing parked vehicles, especially ice cream vans; children are more interested in ice cream than traffic and may run into the road unexpectedly
- needing to cross a pavement, for example to reach or leave a driveway. Give way to pedestrians and cyclists on the pavement
- reversing into a side road; look all around the vehicle and give way to any pedestrians who may be crossing the road
- turning at road junctions; give way to pedestrians who are already crossing the road into which you are turning
- the pavement is closed due to street repairs and pedestrians are directed to use the road
- approaching pedestrians on narrow rural roads without a footway or footpath. Always slow down and be prepared to stop if necessary, giving them plenty of room as you drive past.



**Rule 206: Watch out for children in busy areas**

## 207

Particularly vulnerable pedestrians. These include —

- children and older pedestrians who may not be able to judge your speed and could step into the road in front of you. At 40 mph your vehicle will probably kill any pedestrians it hits. At 20 mph there is only a 1 in 20 chance of a pedestrian being killed. So kill your speed
- older pedestrians who may need more time to cross the road. Be patient and allow them to cross in their own time. Do not hurry them by revving your engine or edging forward
- people with disabilities. People with hearing impairments may not be aware of your vehicle approaching. Those with walking difficulties require more time
- blind or partially sighted people, who may be carrying a white cane and/or using a guide dog. They may not be able to see you approaching
- deafblind people who may be carrying a white cane with a red band or using a dog with a red and white harness. They may not see or hear instructions or signals.

## 208

Near schools. Drive slowly and be particularly aware of young pedal cyclists and pedestrians. In some places there may be a school warning or speed-limit sign which tells you that there may be children crossing the road ahead. Drive very slowly until you are clear of the area.

## 209

Drive carefully and slowly when passing a stationary bus showing a "School Bus" sign (see Annex 9: "Vehicle markings") as children may be getting on or off.

## 210

You **MUST** stop when a school crossing patrol shows a "Stop for children" sign (see Annex 5: "Signals by authorised persons" and Annex 6: "Traffic signs..." —"Signs giving orders")

[Road Traffic Regulation Act 1985, s. 9]

## **Motorcyclists and pedal cyclists**

## 211

It is often difficult to see motorcyclists and pedal cyclists, especially when they are coming up from behind, coming out of junctions, at roundabouts, overtaking you or filtering through traffic. Always look out for them before you emerge from a junction; they could be approaching faster than you think. When turning right across a line of slow-moving or stationary traffic, look out for pedal cyclists or motorcyclists on the inside of the traffic you are crossing. Be especially careful when turning, and when changing direction or lane. Be sure to check mirrors and blind spots carefully.



## 212

When passing motorcyclists and pedal cyclists, give them plenty of room (see Rules 162-167). If they look over their shoulder, it could mean that they intend to pull out, turn right or change direction. Give them time and space to do so.

## 213

Motorcyclists and pedal cyclists may suddenly need to avoid uneven road surfaces and obstacles such as drain covers or oily, wet or icy patches on the road. Give them plenty of room and pay particular attention to any sudden change of direction they may have to make.

## **Other road users**

### **214**

Animals. When passing animals, drive slowly. Give them plenty of room and be ready to stop. Do not scare animals by sounding your horn, revving your engine or accelerating rapidly once you have passed them. Look out for animals being led, driven or ridden on the road and take extra care. Keep your speed down at bends and on narrow country roads. If a road is blocked by a herd of animals, stop and switch off your engine until they have left the road. Watch out for animals on unfenced roads.

### **215**

Horse riders and horse-drawn vehicles. Be particularly careful of horse riders and horse-drawn vehicles, especially when overtaking. Always pass wide and slowly. Horse riders are often children, so take extra care and remember riders may ride in double file when escorting a young or inexperienced horse or rider. Look out for horse riders' and horse drivers' signals and heed a request to slow down or stop. Take great care and treat all horses as a potential hazard; they can be unpredictable, despite the efforts of their rider/driver.

### **216**

Older drivers. Their reactions may be slower than other drivers. Make allowance for this.

### **217**

Learners and inexperienced drivers. They may not be so skilful at anticipating and responding to events. Be particularly patient with learner drivers and young drivers. Drivers who have recently passed their test must display an R-plate and are restricted to a maximum speed of 50 mph (see Annex 10: "Safety code for new drivers").

### **218**

If roads are open to traffic but are being used for play by children or for community events, for example in Home Zones, drive slowly and carefully and be prepared to stop to allow people extra time to make space for you to pass them in safety.

## **Other vehicles**

### **219**

Emergency vehicles. You should look and listen for ambulances, fire engines, police, doctors or other emergency vehicles using flashing blue, red or green lights and sirens or flashing headlights. When one approaches, do not panic. Consider the

route of such a vehicle and take appropriate action to let it pass, while complying with all traffic signs. If necessary, pull to the side of the road and stop, but try to avoid stopping before the brow of a hill, a bend or narrow section of road. Do not endanger yourself, other road users or pedestrians and avoid mounting the kerb. Do not brake harshly on approach to a junction or roundabout as a following vehicle may not have the same view as you.

## 220

Very low-powered vehicles used by disabled people. These small vehicles travel at a maximum speed of 8 mph (see Rules 36-46). If you come upon them, approach with caution.

## 221

Large vehicles. These may need extra road space to turn or to deal with a hazard that you are not able to see. If you are following a large vehicle such as a bus or articulated lorry, be aware that the driver may not be able to see you in the mirrors. Be prepared to stop and wait if it needs room or time to turn.



## 222

Large vehicles can block your view. Your ability to see and to plan ahead will be improved if you pull back to increase your separation distance. Be patient, as larger vehicles are subject to lower speed limits than cars and motorcycles.

## 223

Buses, coaches and trams. Give priority to these vehicles when you can do so safely, especially when they signal to pull away from stops. Look out for people getting off a bus or tram and crossing the road.

## **224**

Electric vehicles. Be careful of electric vehicles such as milk floats and electric cars. They may approach quietly and not be heard.

## **225**

Vehicles with flashing amber beacons. These warn of slow-moving or stationary vehicles (e.g. salt spreaders, snow ploughs or recovery vehicles) or abnormal loads, so approach with caution.

[Road Vehicles Lighting Regulations 1989, reg 20 and Schedule 16]

# **Driving in adverse weather conditions (226-237)**

## **226**

You **MUST** use headlights, not only at night, but also when visibility is seriously reduced, generally when you cannot see for more than 100 metres (328 feet). You may also use front or rear fog lights in seriously reduced visibility but you **MUST** switch them off when visibility improves (see Rule 236).

[Road Vehicles Lighting Regulations 1989 (as modified), regs 25 and 27]

## **227**

Wet weather. In wet weather stopping distances will be at least double those required for stopping on dry roads (see Rule 126). This is because your tyres have less grip on the road. In wet weather —

- you should keep well back from the vehicle in front. This will increase your ability to see and plan ahead
- if the steering becomes unresponsive, it probably means that water is preventing the tyres from gripping the road. Ease off the accelerator and slow down gradually
- the rain and spray from vehicles may make it difficult to see and be seen
- be aware of the dangers of spilt diesel that will make the surface very slippery (see Annex 11: "Vehicle maintenance, safety and security")
- take extra care around pedestrians, pedal cyclists, motorcyclists and horse riders.

## **Icy and snowy weather**

## **228**

In winter check the local weather forecast for warnings of icy or snowy weather. DO NOT drive in these conditions unless your journey is essential. If it is, take great care

and allow more time for your journey. Take an emergency kit of de-icer and ice scraper, torch, warm clothing and boots, first-aid kit, jump leads and a shovel, together with a warm drink and emergency food in case you get stuck or your vehicle breaks down.

## 229

Before you set off —

- you **MUST** be able to see, so clear all snow and ice from all your windows
- you **MUST** ensure that lights are clean and number plates are clearly visible and legible
- make sure the mirrors are clear and the windows are demisted thoroughly
- remove all snow that might fall off into the path of other road users
- check your planned route is clear of delays and that no further snowfalls or severe weather are predicted.

[Road Vehicles (Maintenance and Use) Regulations 2012, reg 8, Road Vehicles Lighting Regulations 1989, reg 23, and Licensing and Registration of Vehicles Act 1985, s. 12(3)]



## 230

When driving in icy or snowy weather —

- drive with care, even if the roads have been treated
- keep well back from the road user in front as stopping distances can be ten times greater than on dry roads
- take care when overtaking vehicles spreading salt or other de-icer, particularly if you are riding a motorcycle or pedal cycle
- watch out for snowploughs, which may throw out snow on either side. Do not overtake them unless the lane you intend to use has been cleared
- be prepared for the road conditions to change over relatively short distances

- listen to travel bulletins and take note of variable message signs that may provide information about weather, road and traffic conditions ahead.

## **231**

Drive extremely carefully when the roads are icy. Avoid sudden actions as these could cause loss of control. You should —

- drive at a slow speed in as high a gear as possible; accelerate and brake very gently
- drive particularly slowly on bends where loss of control is more likely. Brake progressively on the straight before you reach a bend. Having slowed down, steer smoothly round the bend, avoiding sudden actions
- check your grip on the road surface when there is snow or ice by choosing a safe place to brake gently. If the steering feels unresponsive, this may indicate ice and your vehicle losing its grip on the road. When travelling on ice, tyres make virtually no noise.

## **Windy weather**

## **232**

High-sided vehicles are most affected by windy weather, but strong gusts can also blow a car, pedal cyclist, motorcyclist or horse rider off course. This can happen on open stretches of road exposed to strong crosswinds, or when crossing bridges or passing gaps in hedges.

## **233**

In very windy weather your vehicle may be affected by turbulence created by large vehicles. Motorcyclists are particularly affected, so keep well back from them when they are overtaking a high-sided vehicle.

## **Fog**

## **234**

Before entering fog, check your mirrors then slow down. If the word "Fog" is shown on a roadside signal but the road is clear, be prepared for a bank of fog or drifting patchy fog ahead. Even if it seems to be clearing, you can suddenly find yourself in thick fog.

## **235**

When driving in fog you should —

- use your lights as required (see Rule 226)

- keep a safe distance behind the vehicle in front. Rear lights can give a false sense of security
- be able to pull up well within the distance you can see clearly. This is particularly important on motorways and dual carriageways in Great Britain, as vehicles are travelling faster
- use your windscreen wipers and demisters
- beware of other drivers not using headlights
- not accelerate to get away from a vehicle which is too close behind you
- check your mirrors before you slow down. Then use your brakes so that your brake lights warn drivers behind you that you are slowing down
- stop in the correct position at a junction with limited visibility and listen for traffic. When you are sure it is safe to emerge, do so positively and do not hesitate in a position that puts you directly in the path of approaching vehicles.

## 236

You **MUST NOT** use front or rear fog lights unless visibility is seriously reduced (see Rule 226) as they dazzle other road users and can obscure your brake lights. You **MUST** switch them off when visibility improves.

[Road Vehicles Lighting Regulations 1989, reg 27]

## Hot weather

## 237

When driving in hot weather keep your vehicle well ventilated to avoid drowsiness. Be aware that the road surface may become soft or if it rains after a dry spell it may become slippery. These conditions could affect your steering and braking. If you are dazzled by bright sunlight, slow down and if necessary stop.

## Waiting and parking (238-252)

## 238

You **MUST NOT** wait or park on yellow lines during the times of operation shown on nearby time plates (or zone entry signs if in a Controlled Parking Zone). See Annex 6: "Traffic signs..." — "Information signs" and Annex 8: "Road markings". Double yellow lines indicate a prohibition on waiting at any time even if there are no upright signs. You **MUST NOT** wait or park, or stop to set down or pick up passengers, on mandatory school entrance markings (see Annex 8: "Road markings") and you should not do so on advisory school entrance markings.

[Road Traffic Regulation Act 1985, s. 1(12)]

## Parking

**239**

Use off-street parking areas, or bays marked out with white lines on the road as parking places, wherever possible.

If you have to stop on the roadside —

- do not park facing against the traffic flow
- stop as close as you can to the side
- do not stop too close to a vehicle displaying a Blue Badge — remember, the occupant may need more room to get in or out
- you **MUST** switch off the engine, headlights and fog lights
- apply the handbrake before leaving the vehicle
- you **MUST** ensure you do not hit anyone when you open your door — check for cyclists or other traffic
- it is safer for your passengers (especially children) to get out of the vehicle on the side next to the kerb
- put all valuables out of sight and make sure your vehicle is secure
- lock your vehicle.

[Road Vehicles (Maintenance and Use) Regulations 2012, regs 18, 62 and 64, Road Traffic Act 1985, Schedule 2, para 1(5), and Road Vehicles Lighting Regulations 1989, reg 27]



**240**

You **MUST NOT** stop or park on —

- a pedestrian crossing, including the area marked by the zig-zag lines (see Rule 191)
- a clearway (see Annex 6: "Traffic signs..." — "Signs giving orders")
- taxi ranks as indicated by upright signs and markings

- a road marked with double white lines, except to pick up or set down passengers
- the carriageway or the hard shoulder of a motorway in Great Britain except in an emergency (see Rule 270).

[Zebra Pedestrian Crossings Regulations 1972, "Pelican" Pedestrian Crossings Regulations 1990, Puffin Pedestrian Crossings Regulations 2000, Traffic Signs Regulations 2002, regs 10, 26 and 27, Road Traffic Act Regulation Act 1985, s. 1(12), and Road Traffic Act 1985, s. 16]

## 241

You **MUST NOT** park in parking spaces reserved for specific users such as Blue Badge holders, residents or motorcyclists unless entitled to do so.

[Chronically Sick and Disabled Persons Act 1981 and Road Traffic Regulation Act 1985, ss. 1(12), 11(3), and 14B(1)]

## 242

You **MUST NOT** leave your vehicle or trailer in a dangerous position or where it causes any unnecessary obstruction of the road.

[Road Traffic Act 1985, s. 19, and Road Vehicles (Maintenance and Use) Regulations 2012, reg 60]

## 243

**DO NOT** stop or park —

- near a school entrance
- anywhere you would prevent access for the emergency services
- at or near a bus or tram stop or taxi rank
- on the approach to a level crossing
- opposite or within 7 metres (approx 22½ feet) of a junction, except in an authorised parking space
- near the brow of a hill or hump bridge
- opposite a traffic island or (if an obstruction would be caused) another parked vehicle
- where the kerb has been lowered to help wheelchair users and powered mobility vehicles
- in front of an entrance to a property
- on a bend,

except when forced to do so by stationary traffic.

## **244**

You **MUST NOT** park wholly or partly on a pavement without a police constable's permission. Parking on a pavement can obstruct and seriously inconvenience pedestrians, people in wheelchairs or with visual impairments, and people with prams or pushchairs.

[Road Traffic Act 1985, s. 30(2)]

## **245**

Controlled Parking Zones. The zone entry signs indicate the times when the waiting restrictions within the zone are in force. Parking may be allowed in some places at other times. Otherwise parking will be within separately signed and marked bays.

## **246**

Vehicles **MUST NOT** be parked without a police constable's permission on any land situated between two carriageways. The only exception is when parking is essential for loading and unloading, in which case the vehicle **MUST NOT** be left unattended.

[Road Traffic Act 1985, s. 30(2)]

## **247**

Loading and unloading. Do not load or unload where there are yellow markings on the kerb and upright signs advise restrictions are in place (see Annex 8: "Road markings"). Loading and unloading may be permitted where parking is otherwise restricted.

[Road Traffic Regulation Act 1985, s. 1(12)]

## **Parking at night**

## **248**

You **MUST NOT** park without a police constable's permission on a road at night facing against the direction of the traffic flow unless in a recognised parking space.

[Road Vehicles (Maintenance and Use) Regulations 2012, reg 58]

## **249**

All vehicles **MUST** display parking lights when parked —

- on a road with a speed limit greater than 30 mph, or
- on a lay-by on such a road.

[Road Vehicles Lighting Regulations 1989, reg 24 (as modified)]

## **250**

Cars, goods vehicles not exceeding an unladen weight of 1525 kg, invalid carriages, motorcycles and pedal cycles may be parked without lights on a road (or lay-by) with a speed limit of 30 mph or less if they are —

- at least 7 metres (approx 22½ feet) away from any junction, close to the kerb, and facing in the direction of the traffic flow, or
- in a recognised parking place or lay-by.

Other vehicles and trailers, and all vehicles with projecting loads, **MUST NOT** be left on a road at night without lights.

[Road Vehicles Lighting Regulations 1989, reg 24 (as modified) and Road Vehicles (Maintenance and Use) Regulations 2012, reg 85(2) and Schedule 4, para 5(2)]

## **Parking in other circumstances**

### **251**

Parking in fog. It is especially dangerous to park on the road in fog. If it is unavoidable, leave your parking lights or sidelights on.

### **252**

Parking on hills. If you park on a hill you should —

- park close to the kerb and apply the handbrake firmly
- select a forward gear and turn your steering wheel away from the kerb when facing uphill
- select reverse gear and turn your steering wheel towards the kerb when facing downhill
- use "park" if your car has an automatic gearbox.



## Motorways in Great Britain (253-273)

Many other Rules apply to motorway driving. The following, or their GB equivalents, apply either wholly or partly: Rules 46, 57, 83-106, 109-123, 125, 126, 130-134, 139, 144, 146-151, 160, 161, 219, 221, 222, 225-237, 274-278, and 280-290.

### General

#### 253

Prohibited vehicles. Motorways **MUST NOT** be used by pedestrians or holders of provisional motorcycle or car licences. Motorcycles under 50 cc, pedal cyclists, horse riders, certain slow-moving vehicles and those carrying oversized loads (except by special permission) also **MUST NOT** use motorways. Agricultural vehicles and very low-powered wheelchairs or mobility scooters (see Rules 36-46) are also banned from motorways.

#### 254

Traffic on motorways usually travels faster than on other roads, so you have less time to react. It is especially important to use your mirrors earlier and look much further ahead than you would on other roads.

### Motorway signals

#### 255

Motorway signals (see Annex 12: "Light signals controlling traffic") are used to warn you of a danger ahead. For example, there may be an incident, fog, a spillage or road workers on the carriageway that you may not immediately be able to see.

## **256**

Signals situated on the central reservation apply to all lanes. On very busy stretches signals may be overhead with a separate signal for each lane.

## **257**

Amber flashing lights. These warn of a hazard ahead. The signal may show a temporary maximum speed limit, lanes that are closed or a message such as "Fog". Adjust your speed and look out for the danger until you pass a signal which is not flashing or one that gives the "All clear" sign and you are sure it is safe to increase your speed.

## **258**

Red flashing lights. If red lights on the overhead signals flash above your lane and a red "X" is showing, you **MUST NOT** go beyond the signal in that lane. If red lights flash on a signal in the central reservation or at the side of the road, you **MUST NOT** go beyond the signal in any lane.

### **Driving on to the motorway**

## **259**

Joining the motorway. When you join the motorway, you will normally approach it from a road on the left (a slip road) or from an adjoining motorway. You should —

- give priority to traffic already on the motorway
- check the traffic on the motorway and match your speed to fit safely into the traffic flow in the left-hand lane
- not cross solid white lines that separate lanes or use the hard shoulder
- stay on the slip road if it continues as an extra lane on the motorway
- remain in the left-hand lane long enough to adjust to the speed of traffic before considering overtaking.

### **On the motorway**

## **260**

When you can see well ahead and the road conditions are good, you should —

- drive at a steady cruising speed which you and your vehicle can handle safely and is within the GB speed limit
- keep a safe distance from the vehicle in front and increase the gap on wet or icy roads or in fog (see Rules 126 and 235).

## 261

You **MUST NOT** exceed 70 mph or the maximum GB speed limit permitted for your vehicle. If a lower speed limit is in force either permanently or temporarily, for example at road works, you **MUST NOT** exceed the lower limit. On some motorways mandatory signals (which display the speed within a red ring) are used to vary the maximum speed limit to improve traffic flow. You **MUST NOT** exceed this speed limit.

## 262

The monotony of driving on a motorway can make you feel sleepy. To minimise the risk, follow the advice in Rule 91.

## 263

You **MUST NOT** reverse, cross the central reservation, or drive against the traffic flow. If you have missed your exit, or have taken the wrong route, carry on to the next exit.

### **Lane discipline**

## 264

You should always drive in the left-hand lane when the road ahead is clear. If you are overtaking a number of slower-moving vehicles, you should return to the left-hand lane as soon as you are safely past. Slow-moving or speed-restricted vehicles should always remain in the left-hand lane of the carriageway unless overtaking. You **MUST NOT** drive on the hard shoulder except in an emergency or if directed to do so by the police, by Highway Agency traffic officers in uniform, or by signs.

## 265

The right-hand lane of a motorway with three or more lanes **MUST NOT** be used (except in prescribed circumstances) if you are driving —

- any vehicle drawing a trailer
- a goods vehicle with a maximum laden weight exceeding 3.5 tonnes but not exceeding 7.5 tonnes if it is required to be fitted with a speed limiter
- a goods vehicle with a maximum laden weight exceeding 7.5 tonnes
- a passenger vehicle with a maximum laden weight exceeding 7.5 tonnes if it is constructed or adapted to carry more than eight seated passengers in addition to the driver
- a passenger vehicle with a maximum laden weight not exceeding 7.5 tonnes if it is constructed or adapted to carry more than eight seated passengers in addition to the driver and is required to be fitted with a speed limiter.

## 266

When approaching a junction, look well ahead for signals or signs. Direction signs may be placed over the road. If you need to change lanes, do so in good time. At some junctions a lane may lead directly off the motorway. Only get in that lane if you wish to go in the direction indicated on the overhead signs.

## Overtaking

## 267

Do not overtake unless you are sure it is safe and legal to do so. Overtake only on the right. You should —

- check your mirrors
- take time to judge the speeds correctly
- make sure that the lane you will be joining is sufficiently clear ahead and behind
- take a quick sideways glance into the blind spot area to verify the position of a vehicle that may have disappeared from your view in the mirror
- remember that traffic may be coming up behind you very quickly. Check all your mirrors carefully. Look out for motorcyclists. When it is safe to do so, signal in plenty of time and then move out
- ensure you do not cut in on the vehicle you have overtaken
- be especially careful at night and in poor visibility when it is harder to judge speed and distance.

## 268

Do not overtake on the left or move to a lane on your left to overtake. In congested conditions, where adjacent lanes of traffic are moving at similar speeds, traffic in left-hand lanes may sometimes be moving faster than traffic to the right. In these conditions you may keep up with the traffic in your lane even if this means passing traffic in the lane to your right. Do not weave in and out of lanes to overtake.

## 269

Hard shoulder. You **MUST NOT** use the hard shoulder for overtaking. In areas where an Active Traffic Management (ATM) Scheme is in force, the hard shoulder may be used as a running lane. You will know when you can use this because a speed limit sign will be shown above all open lanes, including the hard shoulder. A red cross or blank sign above the hard shoulder means that you **MUST NOT** drive on the hard shoulder except in an emergency or breakdown. Emergency refuge areas have also been built into these areas for use in cases of emergency or breakdown.



**Rule 269: Overhead gantry showing red cross over hard shoulder**

## Stopping

### 270

You **MUST NOT** stop on the carriageway, hard shoulder, slip road, central reservation or verge except in an emergency or when told to do so by the police, Highway Agency traffic officers in uniform, an emergency sign, or flashing red light signals. Do not stop on the hard shoulder either to make or to receive mobile phone calls.

### 271

You **MUST NOT** pick up or set down anyone, or walk on a motorway, except in an emergency.

## Leaving the motorway

### 272

Unless signs indicate that a lane leads directly off the motorway, you will normally leave the motorway by a slip road on your left. You should —

- watch for the signs letting you know you are getting near your exit
- move into the left-hand lane well before reaching your exit
- signal left in good time and reduce your speed on the slip road as necessary.

### 273

When you are leaving the motorway or using a link road between motorways, your speed may be higher than you realise — 50 mph may feel like 30 mph. Check your speedometer and adjust your speed accordingly. Some slip-roads and link roads have sharp bends, so you will need to slow down.

# **Breakdowns and incidents (274-287)**

## **Breakdowns**

**274**

If your vehicle breaks down, think first of all other road users and —

- get your vehicle off the road if possible
- warn other traffic by using your hazard warning lights if your vehicle is causing an obstruction
- help other road users to see you by wearing light-coloured or fluorescent clothing in daylight and reflective clothing at night or in poor visibility
- put a warning triangle on the road at least 45 metres (147 feet) behind your broken-down vehicle on the same side of the road, or use other permitted warning devices if you have them. Always take great care when placing or retrieving them, but never use them on motorways
- if possible, keep your sidelights on if it is dark or visibility is poor
- do not stand (or let anybody else stand) between your vehicle and oncoming traffic
- at night or in poor visibility do not stand where you will prevent other road users seeing your lights.

## **Additional rules for motorways in Great Britain**

**275**

If your vehicle develops a problem, leave the motorway at the next exit or pull into a service area. If you cannot do so, you should —

- pull on to the hard shoulder and stop as far to the left as possible, with your wheels turned to the left
- try to stop near an emergency telephone (situated at approximately one-mile intervals along the hard shoulder)
- leave the vehicle by the left-hand door and ensure your passengers do the same. You **MUST** leave any animals in the vehicle or, in an emergency, keep them under proper control on the verge. Never attempt to place a warning triangle on a motorway
- do not put yourself in danger by attempting even simple repairs
- ensure that passengers keep away from the carriageway and hard shoulder and that children are kept under control
- walk to an emergency telephone on your side of the carriageway (follow the arrows on the posts at the back of the hard shoulder). The telephone is free of charge and connects directly to the Highways Agency or the police. Use these in preference to a mobile phone (see Rule 283). Always face the traffic when you speak on the phone

- give full details to the Highways Agency or the police; also inform them if you are a vulnerable motorist such as disabled, older or travelling alone
- return and wait near your vehicle (well away from the carriageway and hard shoulder)
- if you feel at risk from another person, return to your vehicle by a left-hand door and lock all doors. Leave your vehicle again as soon as you feel this danger has passed.



## 276

Before you rejoin the carriageway after a breakdown, build up speed on the hard shoulder and watch for a safe gap in the traffic. Be aware that other vehicles may be stationary on the hard shoulder.

## 277

If you cannot get your vehicle on to the hard shoulder —

- do not attempt to place any warning device on the carriageway
- switch on your hazard warning lights
- leave your vehicle only when you can safely get clear of the carriageway.

## 278

Disabled drivers. If you have a disability which prevents you from following the above advice, you should —

- stay in your vehicle
- switch on your hazard warning lights
- display a "Help" pennant or, if you have a car or mobile telephone, contact the emergency services and be prepared to advise them of your location.

## **Obstructions**

**279**

If anything falls from your vehicle (or any other vehicle) on to the road, stop and retrieve it only if it is safe to do so.

**280**

Motorways in Great Britain. On a motorway do not try to remove the obstruction yourself. Stop at the next emergency telephone and call the Highways Agency or the police.

## **Incidents**

**281**

Warning signs or flashing lights. If you see or hear emergency vehicles in the distance, be aware there may be an incident ahead (see Rule 219). Police officers and authorised examiners of the Vehicle and Driving Test Centre may be required to work in the carriageway, for example dealing with debris, collisions or conducting rolling road blocks. Police officers will use rear-facing flashing red and blue lights and authorised examiners will use rear-facing flashing red and amber lights in these situations. Watch out for such signals, slow down and be prepared to stop. You **MUST** follow any directions given by police officers as to whether you can safely pass the incident or blockage, and you should observe any such directions given by authorised examiners.

[Road Traffic Act 1985, ss. 16 and 40]

**282**

When passing the scene of an incident or crash, do not be distracted or slow down unnecessarily (for example if an incident is on the other side of the carriageway). This may cause a collision or traffic congestion, but see Rule 283.

**283**

If you are involved in a crash or stop to give assistance —

- use your hazard warning lights to warn other traffic
- ask drivers to switch off their engines and stop smoking
- arrange for the emergency services to be called immediately with full details of the incident location and any casualties (on a motorway in Great Britain use the emergency telephone which allows easy location by the emergency services. If you use a mobile phone, first make sure you have identified your location from the marker posts on the side of the hard shoulder)

- move uninjured people away from the vehicles to safety; on a motorway in Great Britain this should, if possible, be well away from the traffic, the hard shoulder and the central reservation
- do not move injured people from their vehicles unless they are in immediate danger from fire or explosion
- do not remove a motorcyclist's helmet unless it is essential to do so
- be prepared to give first aid as shown in Annex 13: "First aid on the road"
- stay at the scene until emergency services arrive.

If you are involved in any other medical emergency on a motorway, you should contact the emergency services in the same way.

## **Incidents involving dangerous goods**

### **284**

Vehicles carrying dangerous goods in packages will be marked with plain orange reflective plates. Road tankers and vehicles carrying tank containers of dangerous goods will have hazard warning plates (see Annex 9: "Vehicle markings").

### **285**

If an incident involves a vehicle containing dangerous goods, follow the advice in Rule 283 and, in particular, —

- switch off engines and DO NOT SMOKE
- keep well away from the vehicle and do not be tempted to try to rescue casualties as you yourself could become one
- call the emergency services and give as much information as possible about the labels and markings on the vehicle. DO NOT use a mobile phone close to a vehicle carrying inflammable loads.

## **Documentation**

### **286**

If you are involved in an accident which causes damage or injury to any other person, vehicle, animal or property, you **MUST** —

- (a) stop,
- (b) if another person is injured, produce your insurance certificate to any person having reasonable grounds for requiring it, and
- (c) give to any such person your name and address, those of the vehicle owner, and the vehicle's registration number.

[Road Traffic Act 1985, s. 20]

## **287**

If, when reasonably required to do so, you did not give at the time of the accident the particulars mentioned in paragraph (c) of Rule 286, or if another person was injured (irrespective of your giving those particulars at that time), then you **MUST** —

- report the accident, and give those particulars, to the police as soon as possible and in any case within 24 hours, and
- produce to the police your insurance certificate within five days.

[Road Traffic Act 1985, s. 20]

## **Road works, level crossings and tramways (288-308)**

### **Road works**

## **288**

When the "Road Works Ahead" sign is displayed, you will need to be more watchful and look for additional signs providing more specific instructions. Observe all signs — they are there for your safety and the safety of road workers.

- You **MUST NOT** exceed any temporary maximum speed limit.
- Use your mirrors and get into the correct lane for your vehicle in good time and as signs direct.
- Do not switch lanes to overtake queuing traffic.
- Take extra care near pedal cyclists and motorcyclists as they are vulnerable to skidding on grit, mud or other debris at road works.
- Where lanes are restricted due to road works, merge in turn (see Rule 134).
- Do not drive through an area marked off by traffic cones.
- Watch out for traffic entering or leaving the works area, but do not be distracted by what is going on there. Concentrate on the road ahead, not the road works.
- Bear in mind that the road ahead may be obstructed by the works or by slow moving or stationary traffic.
- Keep a safe distance — there could be queues in front.

To obtain further information about road works see Annex 15: "Other information".

[Road Traffic Regulation Act 1985, ss. 3(8) and 24]

### **Additional rules for high-speed roads in Great Britain**

## **289**

Take special care on motorways and other high-speed dual carriageways.

- One or more lanes may be closed to traffic and a lower speed limit may apply.
- Works vehicles that are slow moving or stationary with a large "Keep Left" or "Keep Right" sign on the back are sometimes used to close lanes for repairs, and a flashing light arrow may also be used to make the works vehicle more conspicuous from a distance and give earlier warning to drivers that they need to move over to the next lane.
- Check mirrors, slow down and change lanes if necessary.
- Keep a safe distance from the vehicle in front (see Rule 126).

## 290

Contraflow systems mean that you may be travelling in a narrower lane than normal and with no permanent barrier between you and oncoming traffic. The hard shoulder may be used for traffic, but be aware that there may be broken-down vehicles ahead of you. Keep a good distance from the vehicle ahead and observe any temporary speed limits.

## Level crossings

### 291

A level crossing is where a road crosses a railway line. Approach and cross it with care. Never drive on to a crossing until the road is clear on the other side and do not get too close to the car in front. Never stop or park on, or near, a crossing.

### 292

Overhead electric lines. It is dangerous to touch overhead electric lines. You **MUST** obey the safe height warning road signs and you should not continue forward on to the railway if your vehicle touches any height barrier or bells.

[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, reg 17(5)]

### 293

Controlled crossings on the Manx Steam Railway. For the most part such crossings have traffic light signals with a steady amber light, twin flashing red stop lights (see Annex 12: "Light signals controlling traffic" and Annex 6: "Traffic signs..." — "Warning signs") and an audible alarm for pedestrians. They may have full, half or no barriers.

- You **MUST** always obey the flashing red stop lights.
- You **MUST** stop behind the white line across the road.
- Keep going if you have already crossed the white line when the amber light comes on.
- Do not reverse on to or over a controlled crossing.
- You **MUST** wait if a train goes by and the red lights continue to flash. This means another train will be passing soon.

- Only cross when the lights go off and barriers open.
- Never zig-zag around half-barriers; they lower automatically because a train is approaching
- At crossings where there are no barriers, a train is approaching when the lights show.



[Road Traffic Act 1985, s. 16, and Traffic Signs Regulations 2002, regs 10, 39 and 40]

## 294

Railway telephones in Great Britain. If you are driving a large or slow-moving vehicle, a long and low vehicle with a risk of grounding, or herding animals, a train could arrive before you are clear of the crossing. You **MUST** obey any sign in Great Britain instructing you to use the railway telephone to obtain permission to cross. You **MUST** also telephone when clear of the crossing if requested to do so.

[GB Road Traffic Act 1988, s.36, and Traffic Signs Regulations 2002, regs 10 and 16(1)]

## 295

Crossings without traffic lights. Vehicles should stop and wait at the barrier or gate when it begins to close and not cross until the barrier or gate opens.

## 296

User-operated gates or barriers in Great Britain. Some crossings there have "Stop" signs and small red and green lights. You **MUST NOT** cross when the red light is showing; only cross if the green light is on. If crossing with a vehicle, you should —

- open the gates or barriers on both sides of the crossing
- check that the green light is still on and cross quickly
- close the gates or barriers when you are clear of the crossing.

[GB Road Traffic Act 1988, s. 36, and Traffic Signs Regulations 2002, regs 10 and 52]

## **297**

If there are no lights, follow the procedure in Rule 295. Stop, look both ways and listen before you cross. In Great Britain, if there is a railway telephone, always use it to contact the signal operator to make sure it is safe to cross. Inform the signal operator again when you are clear of the crossing.

## **298**

Open crossings, particularly on the Manx Electric Railway. These have no gates, barriers or attendant but may have traffic lights, a "Give Way" sign or a level crossing sign. If there are traffic lights, comply with them, otherwise look both ways, listen and make sure there is no train coming before you cross.

## **299**

Incidents and breakdowns. If your vehicle breaks down, or if you have an incident, on a crossing you should —

- get everyone out of the vehicle and clear of the crossing immediately
- if available in Great Britain, use a railway telephone to tell the signal operator. Follow the instructions you are given
- move the vehicle clear of the crossing if there is time before a train arrives. If the alarm sounds, or the amber light comes on, leave the vehicle and get clear of the crossing immediately.

## **Horse-drawn trams**

## **300**

Take extra care where trams run along the road. Vehicles fitted with narrow tyres should avoid driving directly on top of the rails.

## **301**

Always give way to trams, particularly when turning across the tracks. Overtake only on the left of a tram. If driving a large or heavy vehicle, do not follow too closely to a tram.

## **302**

Do not stop on any part of a tram track. Remember that a tram cannot steer round an obstruction.

### **303**

Tram stops. Do not drive between a tram and the left-hand kerb when a tram has stopped to pick up passengers. Before crossing the road to board a tram, look carefully. Before alighting from a tram, ALWAYS LOOK REARWARDS in the direction from which the tram has come. Be aware that conductors may alight at tram stops to assist passengers by stopping or directing traffic.

### **304**

Look out for pedestrians, especially children, running to catch a tram.

### **305**

Always give priority to trams, especially when they pull away from stops, unless it would be unsafe to do so. Look out for people getting off a bus or tram and crossing the road.

### **306**

All road users, but particularly pedal cyclists and motorcyclists, should take extra care when driving or riding close to or crossing the tracks, especially if the rails are wet. You should take particular care when crossing the rails at shallow angles. It is safer to cross more directly.

### **307**

Trams **MUST** give way to persons lawfully using a pedestrian crossing which crosses the tracks.

## **Tramways in Great Britain**

### **308**

Overhead electric lines. Tramway overhead wires in Great Britain are normally 5.8 metres above any carriageway, but can be lower. You should ensure that you have sufficient clearance between the wire and your vehicle (including any load you are carrying) before driving under an overhead wire. Drivers of vehicles with extending cranes, booms, tipping apparatus or other types of variable height equipment should ensure that the equipment is fully lowered. Where overhead wires are set lower than 5.8 metres, these will be indicated by height clearance markings — similar to "low bridge" signs. The height clearances on these plates should be carefully noted and observed. If you are in any doubt as to whether your vehicle will pass safely under the wires, you should always contact the local police or the tramway operator. Never take a chance as this can be extremely hazardous.

## **ANNEX 1**

### **YOU AND YOUR PEDAL CYCLE**

Make sure that you feel confident of your ability to ride safely on the road. Be sure that —

- you choose the right size and type of cycle for comfort and safety
- lights and reflectors are kept clean and in good working order
- tyres are in good condition and inflated to the pressure shown on the tyre
- gears are working correctly
- the chain is properly adjusted and oiled
- the saddle and handlebars are adjusted to the correct height.

You should fit a bell to your pedal cycle.

You **MUST** —

- ensure your brakes are efficient
- at night, use lit front and rear lights, whether flashing or not, and have a red rear reflector.

[Brakes on Pedal Cycles Regulations 1991, regs 2 to 6, and Road Vehicles Lighting Regulations 1989, reg 18 and Schedule 1, Table III]

Cycle training can help both children and adults, especially those adults returning to cycling, to develop the skills needed to cycle safely on today's roads.

All pedal cyclists should consider the benefits of undertaking cycle training.

## ANNEX 2

# SIGNALS TO OTHER ROAD USERS

### Direction indicator signals



I intend to move out to the right or turn right



I intend to move in to the left or turn left or stop on the left

### Brake light signals



I am applying the brakes



### Reversing light signals



I intend to reverse

These signals should not be used except for the purposes described.

### Arm signals

For use when direction indicator signals are not used, or when necessary to reinforce direction indicator signals and stop lights. **Also for use by pedal cyclists and those in charge of horses.**



I intend to move in to the left or turn left



I intend to move out to the right or turn right



I intend to slow down or stop



## **ANNEX 3**

# **MOTORCYCLE LICENCE REQUIREMENTS**

### **Mopeds**

1. A moped is a motor cycle (with or without a sidecar) which is propelled either by electric power or by an internal combustion engine with a cylinder capacity not exceeding 50 cc and which has a maximum design speed not exceeding 28 mph (45 km/h).
2. If you wish to begin driving a moped on public roads, you **MUST** have a provisional moped licence and have successfully completed a Compulsory Basic Training (CBT) course. To go on to obtain your full moped licence you **MUST** pass a theory test and then the moped practical test. If you do not obtain such a licence within two years, you **MUST** successfully retake a CBT course and do so every two years until the full licence is obtained. While a learner driver you **CANNOT** drive a moped without L-plates.
3. If you have a full car licence for which you passed your driving test before 1st July 2007, the licence carries with it a full entitlement to drive on public roads any class of mopeds. If you have a full car licence for which you passed your driving test on or after that date, the licence carries with it a full entitlement to drive on public roads only mopeds with more than two wheels.

### **Motorcycles**

4. The machines that may be driven with a provisional licence are —
  - a learner motorcycle if it is a solo machine, and
  - any class of motorcycle if it is attached to a sidecar.
5. A learner motorcycle is a motor bicycle (not being a moped) which has no sidecar attached and which is propelled either by electric power or by an internal combustion engine having a cylinder capacity not exceeding 125 cc and a maximum net power output not exceeding 11 kilowatts.
6. If you have a provisional motorcycle licence, you **MUST** successfully complete a CBT course. You can then drive on public roads with L-plates for up to two years. If you do not obtain your full motorcycle licence within this period, you **MUST** successfully retake a CBT course and do so every two years until the full licence is obtained. To obtain your full motorcycle licence you **MUST** pass a motorcycle theory test and then a practical test.
7. There are two categories of full licence that you may seek to attain: a light motorcycle licence (category A1), which authorises you to drive machines of the same class as learner motorcycles, or a standard motorcycle licence (category A),

which authorises you to drive machines of all classes. To obtain an A1 licence you **MUST** pass the practical test on a learner motorcycle which has an engine with a cylinder capacity of at least 75 cc. To obtain an A licence you **MUST** pass the practical test on a learner motorcycle which has an engine with a cylinder capacity of at least 121 cc and which is capable of an unassisted speed of 100 km/h on the level.

8. If you pass your practical test on a machine with a sidecar, you will be restricted by your full A1 or A licence to driving only machines with sidecars.

## **Upgrading**

9. If you have a full moped licence and wish to obtain full motorcycle entitlement, you **MUST** first successfully complete a CBT course relating to your motorcycle even though you have previously passed a CBT course relating to any or every class of mopeds. Such a course **MUST** be successfully retaken every two years if you do not obtain your full motorcycle entitlement before the end of such a period. You **MUST** go on to pass a motorcycle theory test unless —

- your full moped licence authorises you to drive two-wheeled mopeds or mopeds of all classes, and
- you passed with respect to them your driving test on or after 1st July 2000.

You **MUST** then pass a practical motorcycle test.

10. If you hold an A1 licence and wish to obtain an A licence, you are exempted from taking the motorcycle theory test.

## **Pillion passengers and trailers**

11. Learner drivers on solo machines **MUST NOT** carry a pillion passenger or draw a trailer.

[Moped and Learner Motor Cycle (Definitions) Regulations 2003, Driving Licences and Tests Regulations 2007, regs 3(1), 8(1)(b) and (2), 11, 20, 24, 28(1)(d) and Schedules 2, 7 and 10, Motor Bicycles (Approved Training Courses for Drivers) Regulations 2002, reg 15, Driving Licences and Tests (Amendment) Regulations 1999, Schedule 1, and Road Traffic Act 1985 (as amended) Schedule 3, paras 2 and 6(2C) to (2E)]

## **ANNEX 4**

# **THE ROAD USER AND THE LAW**

### **Road traffic law**

1. Legal requirements are indicated in the Code by the use of the words "**MUST**" and "**MUST NOT**". Please read what the Foreword to the Code has to say, not only about them, but also about the other advice set out in the Code.
2. The legislative provisions containing the legal requirements are cited in brackets throughout the Code. If you wish to know the precise wording of the law, please refer to them.
3. A road is defined in law as being "any highway and any other road to which the public has access, and includes a footway or a verge forming part of a road, and a bridge over which a road passes" (Road Traffic Act 1985, s. 72(1)). Although a road may therefore embrace a footpath, bridle-path, cycle-path or cycle track, the expression is generally used in the Code to refer to an all-purpose highway containing a carriageway for vehicles, whether or not it contains a footway [pavement] for pedestrians.

### **Acts, Regulations, and Orders**

4. Acts of Tynwald (primary legislation) passed after 1st January 2001, and Regulations or Orders (secondary legislation) enacted after 1st January 2000, may be viewed on the IOM Government website ([www.gov.im/infocentre/acts](http://www.gov.im/infocentre/acts)) by clicking on to the relevant links. If you wish to see or require a copy of legislation passed or enacted prior to those dates, please contact the Tynwald Information Service on (01624) 685520 or by e-mail to [library@tynwald.org.im](mailto:library@tynwald.org.im).
5. The Road Vehicles Lighting Regulations 1989 and the Traffic Signs Regulations 2002, both of which are GB Regulations applied with modifications to the Isle of Man, may be viewed on the UK Government website ([www.legislation.gov.uk/uksi](http://www.legislation.gov.uk/uksi)) and may be purchased from the Stationery Office in England or through any good bookseller.

### **Penalties**

6. The Road Traffic Act 1985 (as amended) sets out the maximum penalties for road traffic offences. The seriousness of an offence is reflected in the maximum penalties. It is for a court to decide what sentence to impose according to the circumstances of a case.
7. Where a court has power to impose discretionary disqualification, it may, if it decides to do so, disqualify for such period as it thinks fit (usually only a few months at most) with the disqualification being endorsed on the licence. If a court decides

not to impose discretionary disqualification, it **MUST**, unless special reasons obtain, endorse on the licence the penalty points prescribed in the Act according to the fixed number or range set out there. A driver who accumulates 12 or more penalty points within a three-year period **MUST**, unless special reasons obtain, be disqualified. The period will be a minimum of six months but will be appreciably longer if the driver has been disqualified on one or more occasions within the previous three years.

8. In the case of serious offences such as dangerous driving or certain drink-drive offences, a court **MUST**, unless special reasons obtain, disqualify. Except where indicated in the table below, the period of disqualification is a minimum of 12 months but in the case of serious drink-drive offences it can be as high as a minimum of five years where gross alcoholic impairment is involved.

10. Where a driver is convicted of an offence involving disqualification, a court **MUST** order the driver to take an extended driving test if —

- the offence is subject to obligatory disqualification and the court disqualifies
- the driver has been convicted of the same or a similar offence within the previous three years
- the offence was committed by a newly qualified driver, or
- the court considers the facts of the case raise a reasonable doubt as to the driver's competence to drive.

Where a driver is convicted in other circumstances of an offence involving disqualification, a court has discretion to order that an extended driving test be taken.

#### **CERTAIN DRIVING OFFENCES (many more are omitted for the sake of conciseness)**

<b>Offence</b>	<b>Maximum penalties</b>	<b>Disqualification</b>	<b>Endorsement</b>	<b>Penalty points</b>
Causing death by dangerous driving	14 years' imprisonment or an unlimited fine or both	Obligatory (not less than 2 years)	Obligatory	—
Causing serious bodily harm by dangerous driving	5 years' imprisonment or an unlimited fine or both	Obligatory (not less than 2 years)	Obligatory	—
Dangerous driving	2 years' imprisonment or an unlimited fine or both	Obligatory	Obligatory	—
Causing death by careless driving when under the influence of drink or drugs	14 years' imprisonment or an unlimited fine or both	Obligatory	Obligatory	—

Causing death by careless or inconsiderate driving	5 years' imprisonment or an unlimited fine or both	Obligatory	Obligatory	—
Causing serious bodily harm by careless or inconsiderate driving	2 years' imprisonment or an unlimited fine or both	Obligatory	Obligatory	—
Careless or inconsiderate driving	£5,000	Discretionary	Obligatory	3-9
Causing death by driving when unlicensed, disqualified or uninsured	5 years' imprisonment or an unlimited fine or both	Obligatory (not less than 2 years)	Obligatory	—
Driving under age	£1,000	Discretionary	Obligatory	2
Driving when unfit to drive through drink or drugs	2 years' imprisonment or an unlimited fine or both	Obligatory	Obligatory	10
Being in charge of a vehicle when unfit to drive through drink or drugs	6 months' imprisonment or £5,000 or both	Discretionary	Obligatory	10
Driving with excess alcohol in breath, blood or urine	6 months' imprisonment or £5,000 or both	Obligatory	Obligatory	10
Being in charge of a vehicle with excess alcohol in breath, blood or urine	3 months' imprisonment or £2,500 or both	Discretionary	Obligatory	10
Failing to provide specimen for analysis required to ascertain ability to drive or proportion of alcohol when driving or attempting to drive	2 years' imprisonment or an unlimited fine or both	Obligatory (not less than 5 years)	Obligatory	10
Failing to stop after accident and give particulars or report it	9 months' imprisonment or £5,000 or both	Discretionary	Obligatory	5-10
Driving while disqualified	6 months' imprisonment or £5,000 or both	Discretionary	Obligatory	6
Failing to notify onset of a disability	£1,000	—	—	—
Using a motor vehicle when uninsured	£5,000	Discretionary	Obligatory	4-8
Using a vehicle in a dangerous condition	(a) £2,500 in the case of a goods vehicle	Discretionary	Obligatory	3

	(b) £1,000 otherwise			
Driving a vehicle when the condition of any window obscures the vision of the driver	(a) £2,500 in the case of a goods vehicle (b) £1,000 otherwise	Discretionary	Obligatory	3
Failing to have proper control of a vehicle or a full view of the road and traffic ahead	£1,000	—	—	—
Driving a motor vehicle while holding a mobile phone in a hand	£2,500, including forfeiture of phone	Discretionary	Obligatory	4
Driving without or contrary to a driving licence	£1,000	Discretionary in certain circumstances	Obligatory in those circumstances	2
Speeding	£1,000	Discretionary	Obligatory	1-10
Failing to comply with traffic lights or signs or with the directions of a constable	£1,000	Discretionary	Obligatory	3
Contravening seat-belt wearing requirements	£500	—	—	—
Dangerous pedal cycling	£2,500	—	—	—
Careless or inconsiderate pedal cycling	£1,000	—	—	—
Pedal cycling when unfit through drink or drugs	£2,500	—	—	—
Driving a vehicle on a pavement	£500	—	—	—
Failing to identify the driver of a vehicle	(a) £2,500 when involving a motor vehicle (b) £1,000 when involving a pedal cycle	—	—	—
Failing to stop when required by a constable	(a) £5,000 when involving a motor vehicle (b) £1,000 when involving a pedal cycle or animal	—	—	—

## **Newly qualified drivers**

10. For a period of 12 months from gaining their full licence newly qualified drivers (including those required by a court to take an extended driving test) **MUST** display an R-plate on the front and back of their vehicle and **MUST NOT** exceed a speed of 50 mph. If convicted of a driving offence and disqualified during this period, such drivers will be subject to the same restrictions for a period of 24 months commencing on the date on which their disqualification ends.

## **Seizure, retention, and disposal of vehicles**

11. The police may seize and remove to storage a vehicle which they suspect has been driven on roads in circumstances where the driver —

- was uninsured,
- failed to display on the vehicle a current vehicle licence (tax disc),
- was disqualified,
- held no current driving licence,
- is a learner driver and was not accompanied by a qualified driver, or
- was committing a serious driving offence connected with drink or drugs.

12. If the driver is to be prosecuted, disposal of the vehicle will be decided by the court in the event that he or she is convicted. In the case of a conviction for not displaying a current vehicle licence, the vehicle will be returned by order of the court to the owner. In other cases the court has absolute discretion to order the vehicle's return, sale or forfeiture. If it is ordered to be sold, the proceeds of sale will be paid to the owner minus the police's costs of seizure and storage. Special arrangements apply in relation to a vehicle which is the subject of a hiring or hire-purchase agreement.

13. If no prosecution is brought or if the driver is found not guilty, the vehicle will be returned to the owner.

## **Insurance**

14. In addition to the penalties that a court may decide to impose the cost of insurance is likely to rise considerably after conviction for a serious driving offence. This is because insurance companies consider the drivers in question are more likely to be involved in a collision.

## **ANNEX 5**

# **SIGNALS BY AUTHORISED PERSONS**

### **Police officers**

#### **Stop**



Traffic approaching from the front

Traffic approaching from both front and behind

Traffic approaching from behind

#### **To beckon traffic on**



From the side

From the front

From behind\*

### **Arm signals to persons controlling traffic**



I want to go straight on



I want to turn left;  
use either hand



I want to turn right

## School Crossing Patrols



Not ready to cross pedestrians



Barrier to stop pedestrians crossing



Ready to cross pedestrians, vehicles must be prepared to stop



All vehicles must stop

## ANNEX 6

### TRAFFIC SIGNS ENCOUNTERED IN THE ISLE OF MAN OR GREAT BRITAIN

#### Signs giving orders

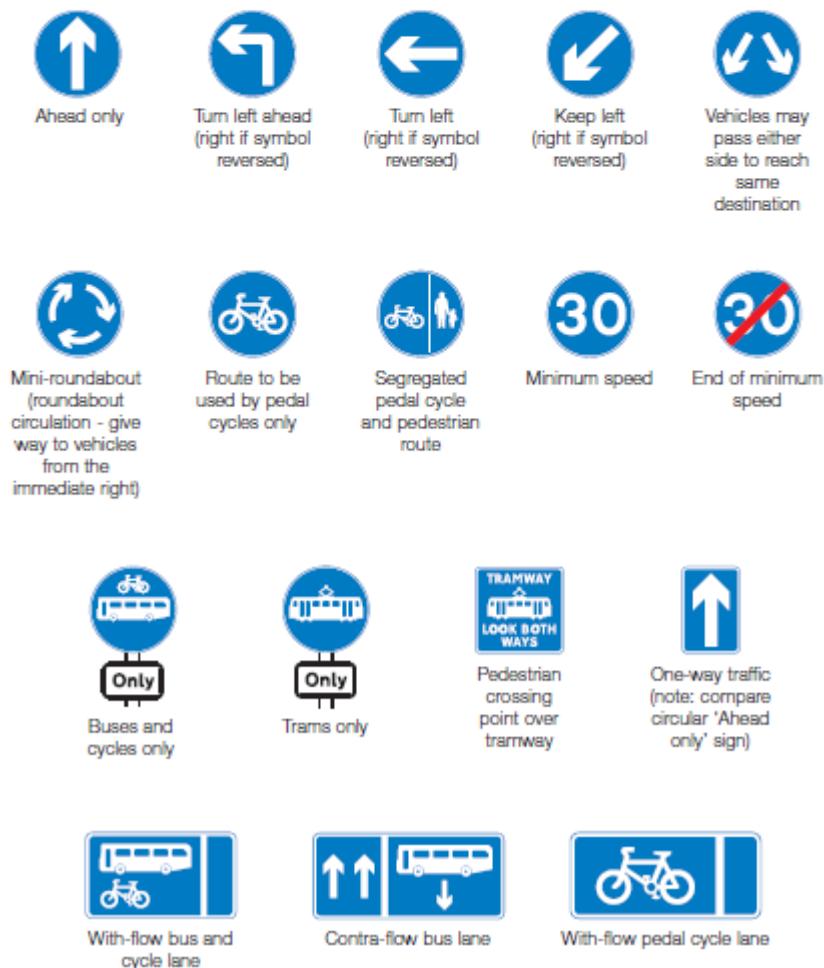
Signs with red circles are mostly prohibitive.  
Plates below signs qualify their message.



**Note:** Although this Code shows many of the signs commonly used in the Isle of Man or Great Britain, a comprehensive explanation of the joint signing system is given in the GB Department for Transport's booklet "Know Your Traffic Signs", which may be purchased through all good booksellers. The booklet also illustrates and explains the vast majority of signs likely to be encountered either here or there. Some older designs of signs may still be seen on roads.



Signs with blue circles but no red border mostly give positive instruction.



## Warning signs

Mostly triangular



Distance to  
'STOP' line  
ahead



Dual  
carriageway  
ends



Road  
narrows on  
right (left if  
symbol reversed)



Road  
narrows on  
both sides



Distance to  
'Give Way'  
line ahead



Crossroads



Junction on  
bend ahead



T-junction with  
priority over  
vehicles from  
the right



Staggered  
junction



Traffic merging  
from left ahead

The priority through route is indicated by the broader line.



Double bend first  
to left (symbol  
may be reversed)



Bend to right  
(or left if symbol  
reversed)



Roundabout



Uneven road



Plate below  
some signs



Two-way  
traffic crosses  
one-way road



Two-way traffic  
straight ahead



Opening or  
swing bridge  
ahead



Low-flying aircraft  
or sudden  
aircraft noise



Falling or  
fallen rocks



Traffic signals  
not in use



Traffic signals



Slippery road



Steep hill  
downwards



Steep hill  
upwards

Gradients may be shown as a ratio i.e. 20% = 1:5



Tunnel ahead



Trams  
crossing  
ahead



Level crossing  
with barrier or  
gate ahead

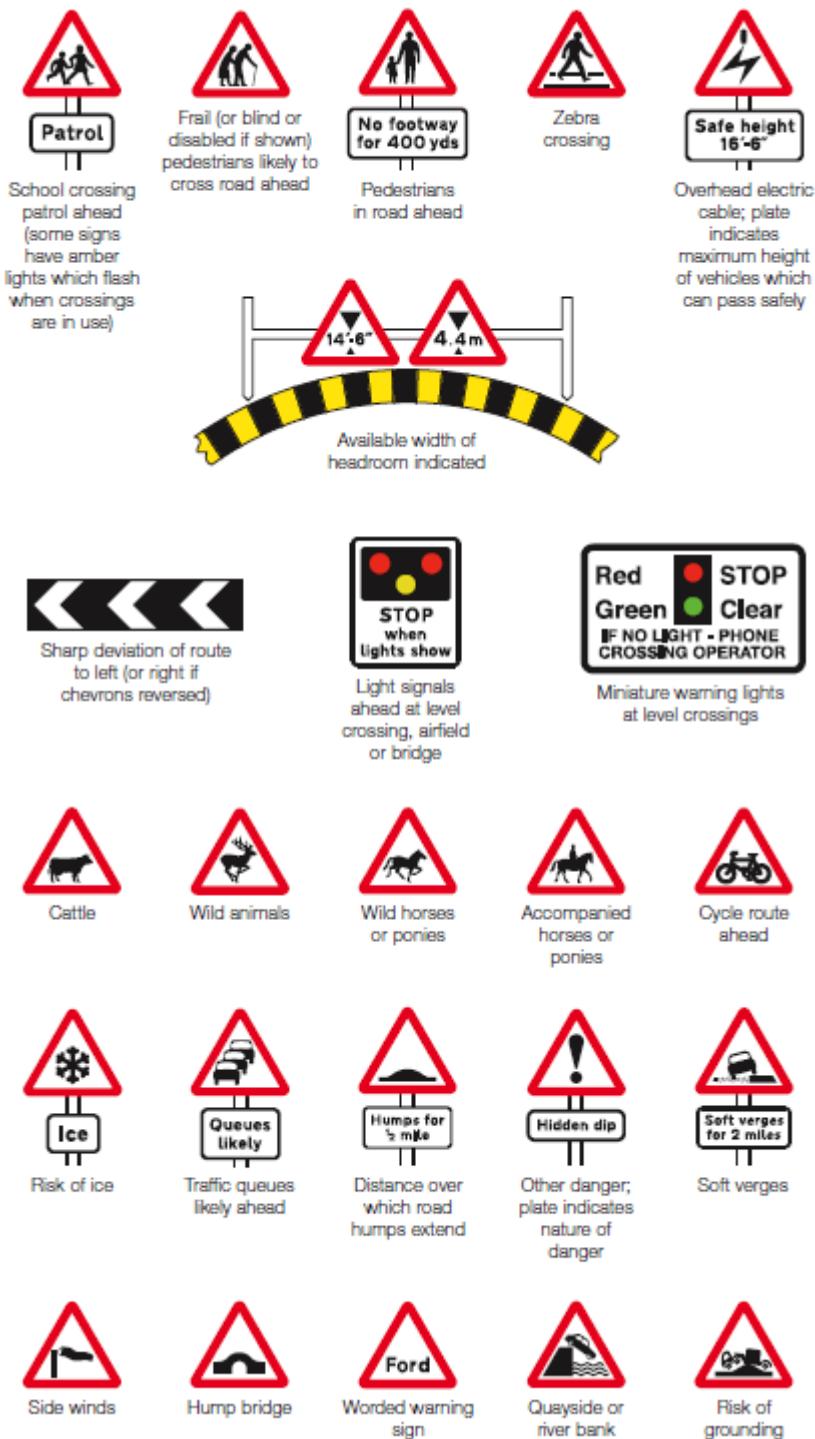


Level crossing  
without barrier  
or gate ahead



Level crossing  
without barrier

### Warning signs - continued



## Direction signs

Mostly rectangular

Signs on motorways - blue backgrounds



At a junction leading directly into a motorway (junction number may be shown on a black background)



On approaches to junctions (junction number on black background)



Route confirmatory sign after junction

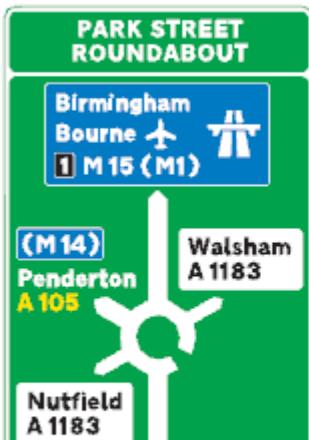


Downward pointing arrows mean 'Get in lane'  
The left-hand lane leads to a different destination from the other lanes.



The panel with the inclined arrow indicates the destinations which can be reached by leaving the motorway at the next junction

## Signs on primary routes - green backgrounds



On approaches to junctions



At the junction



Route confirmatory sign after junction



On approaches to junctions



On approach to a junction in Wales (bilingual)



- Blue panels indicate that the motorway starts at the junction ahead.
- Motorways shown in brackets can also be reached along the route indicated.
- White panels indicate local or non-primary routes leading from the junction ahead.
- Brown panels show the route to tourist attractions.
- The name of the junction may be shown at the top of the sign.
- The aircraft symbol indicates the route to an airport.
- A symbol may be included to warn of a hazard or restriction along that route.

## Green background signs - continued



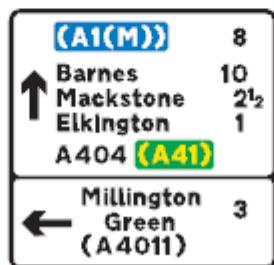
Primary route forming part of a ring road



## Signs on non-primary and local routes - black borders



On approaches to junctions



At the junction



Direction to toilets with access for the disabled



Green panels indicate that the primary route starts at the junction ahead.  
Route numbers on a blue background show the direction to a motorway.  
Route numbers on a green background show the direction to a primary route.

## Other direction signs



Picnic site



Ancient monument in the care of English Heritage



Direction to a car park



Tourist attraction



Direction to camping and caravan site



Advisory route for lorries



Route for pedal cycles forming part of a network



Recommended route for pedal cycles to place shown



Route for pedestrians



Symbols showing emergency diversion route for motorway and other main road traffic



Diversion route

## Information signs

All rectangular



Entrance to controlled parking zone



Entrance to congestion charging zone



End of controlled parking zone



Advance warning of restriction or prohibition ahead



Parking place for solo motorcycles



With-flow bus lane ahead which pedal cycles and taxis may also use



Lane designated for use by high occupancy vehicles (HOV) - see rule 142



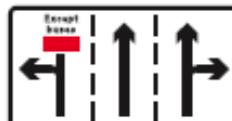
Vehicles permitted to use an HOV lane ahead



End of motorway



Start of motorway and point from which motorway regulations apply



Appropriate traffic lanes at junction ahead



Traffic on the main carriageway coming from right has priority over joining traffic



Additional traffic joining from left ahead. Traffic on main carriageway has priority over joining traffic from right hand lane of slip road



Traffic in right hand lane of slip road joining the main carriageway has priority over left hand lane



'Countdown' markers at exit from motorway (each bar represents 100 yards to the exit). Green-backed markers may be used on primary routes and white-backed markers with black bars on other routes. At approaches to concealed level crossings white-backed markers with red bars may be used. Although these will be erected at equal distances the bars do not represent 100 yard intervals.



Motorway service area sign showing the operator's name

## Information signs - continued



Traffic has priority over oncoming vehicles



Hospital ahead with Accident and Emergency facilities



Tourist information point



No through road for vehicles



Recommended route for pedal cycles



Home Zone



Area in which cameras are used to enforce traffic regulations



Bus lane on road at junction ahead

## Road works signs



Road works



Loose chippings



Temporary hazard at road works



Temporary lane closure  
(the number and position of arrows and red bars may be varied according to lanes open and closed)



Slow-moving or stationary works vehicle blocking a traffic lane. Pass in the direction shown by the arrow.



Mandatory speed limit ahead



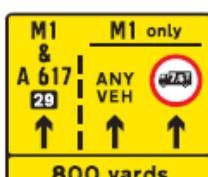
Road works 1 mile ahead



End of road works and any temporary restrictions including speed limits



Signs used on the back of slow-moving or stationary vehicles warning of a lane closed ahead by a works vehicle. There are no cones on the road.



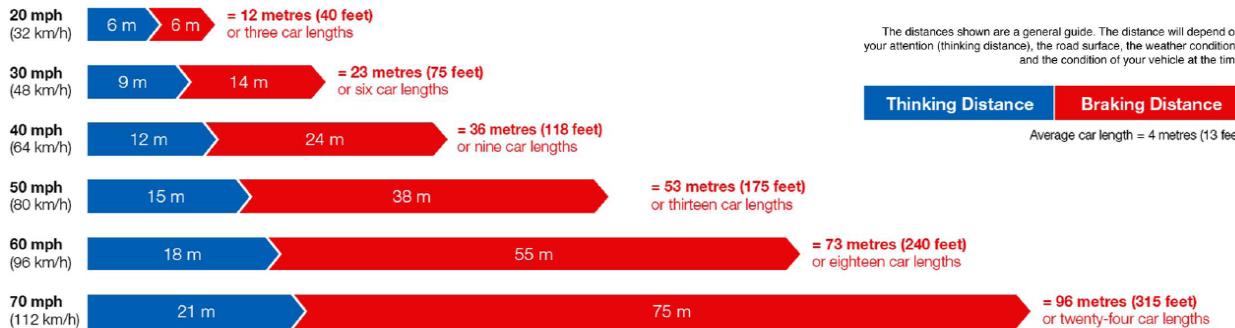
Lane restrictions at road works ahead



One lane crossover at contraflow road works

# ANNEX 7

## Typical Stopping Distances



## ANNEX 8

### Road markings

#### Across the carriageway



Stop line at signals or  
police control



Stop line at 'Stop' sign



Stop line for pedestrians  
at a level crossing



Give way to traffic on major road  
(can also be used at  
mini roundabouts)



Give way to traffic from the right  
at a roundabout



Give way to traffic from the right  
at a mini-roundabout

#### Along the carriageway



Edge line



Centre line  
See Rule 127



Hazard  
warning line  
See Rule 127



Double white lines  
See Rules 128 and 129



See Rule 130

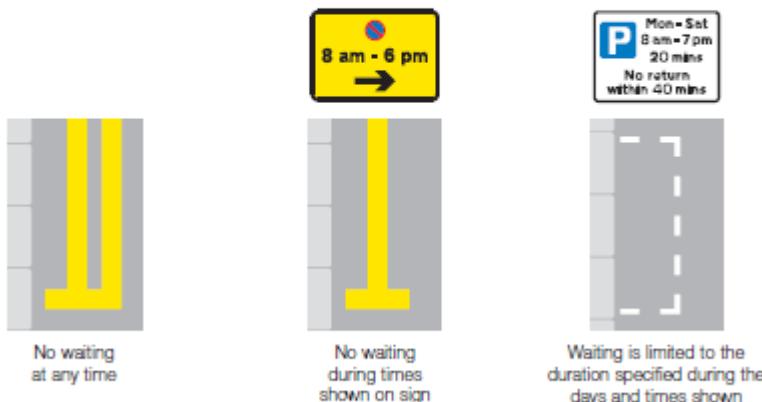


Lane line See  
Rule 131

## Along the edge of the carriageway

### Waiting restrictions

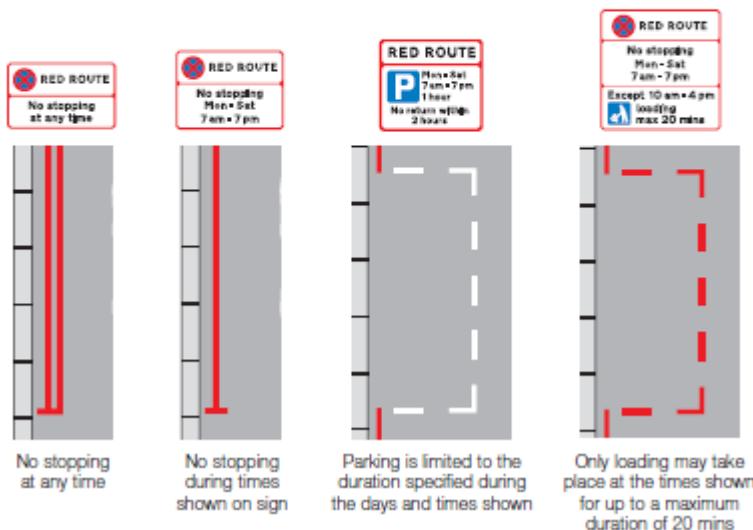
Waiting restrictions indicated by yellow lines apply to the carriageway, pavement and verge. You may stop to load or unload (unless there are also loading restrictions as described below) or while passengers board or alight. Double yellow lines mean no waiting at any time, unless there are signs that specifically indicate seasonal restrictions. The times at which the restrictions apply for other road markings are shown on nearby plates or on entry signs to controlled parking zones. If no days are shown on the signs, the restrictions are in force every day including Sundays and Bank Holidays. White bay markings and upright signs (see below) indicate where parking is allowed.



### Red Route stopping controls

Red lines are used on some roads instead of yellow lines. In London the double and single red lines used on Red Routes indicate that stopping to park, load/unload or to board and alight from a vehicle (except for a licensed taxi or if you hold a Blue Badge) is prohibited. The red lines apply to the carriageway, pavement and verge. The times that the red line prohibitions apply are shown on nearby signs, but the double red line ALWAYS means no stopping at any time. On Red Routes you may stop to park, load/unload in specially marked boxes and adjacent signs specify the times and purposes and duration allowed. A box MARKED IN RED indicates that it may only be available for the purpose specified for part of the day (eg between busy peak periods). A box MARKED IN WHITE means that it is available throughout the day.

RED AND SINGLE YELLOW LINES CAN ONLY GIVE A GUIDE TO THE RESTRICTIONS AND CONTROLS IN FORCE AND SIGNS, NEARBY OR AT A ZONE ENTRY, MUST BE CONSULTED.



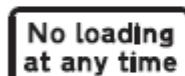
## On the kerb or at the edge of the carriageway

### Loading restrictions on roads other than Red Routes

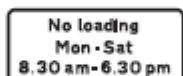
Yellow marks on the kerb or at the edge of the carriageway indicate that loading or unloading is prohibited at the times shown on the nearby black and white plates. You may stop while passengers board or alight. If no days are indicated on the signs the restrictions are in force every day including Sundays and Bank Holidays.

ALWAYS CHECK THE TIMES SHOWN ON THE PLATES.

Lengths of road reserved for vehicles loading and unloading are indicated by a white 'bay' marking with the words 'Loading Only' and a sign with the white on blue 'trolley' symbol. This sign also shows whether loading and unloading is restricted to goods vehicles and the times at which the bay can be used. If no times or days are shown it may be used at any time. Vehicles may not park here if they are not loading or unloading.



No loading or unloading at any time



No loading or unloading at the times shown



Loading bay

## Other road markings



Keep entrance clear of stationary vehicles, even if picking up or setting down children



Warning of 'Give Way' just ahead



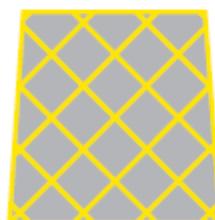
Parking space reserved for vehicles named



See Rule 243



See Rule 141



Box junction - See Rule 174



Do not block that part of the carriageway indicated



Indication of traffic lanes

## ANNEX 9

### Vehicle markings

#### Large goods vehicle rear markings

Motor vehicles over 7500 kilograms maximum gross weight and trailers over 3500 kilograms maximum gross weight



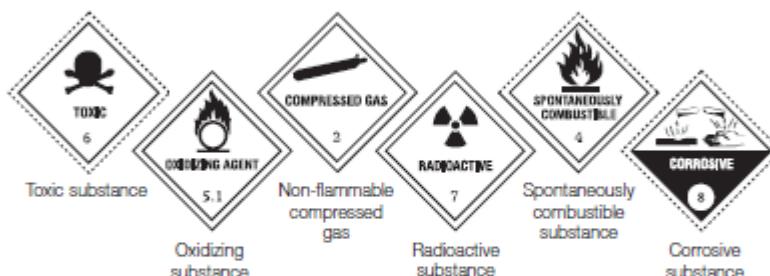
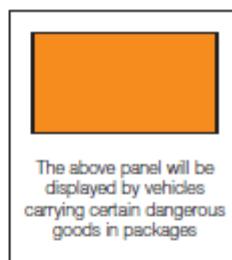
The vertical markings are also required to be fitted to builders' skips placed in the road, commercial vehicles or combinations longer than 13 metres (optional on combinations between 11 and 13 metres)

#### Hazard warning plates

Certain tank vehicles carrying dangerous goods must display hazard information panels



The panel illustrated is for flammable liquid.  
Diamond symbols indicating other risks include:



#### Projection markers



Both required when load or equipment (eg crane jib) overhangs front or rear by more than two metres



#### Other



School bus  
(displayed in front or rear window of bus or coach)

## **ANNEX 10**

### **SAFETY CODE FOR NEW DRIVERS**

Once you have passed the driving test you will be able to drive on your own. This will provide you with lots of opportunities but you need to remain safe. Even though you have shown you have the skills you need to drive safely, many newly qualified drivers lack experience. You need to continue to develop your skills, especially anticipating other road users' behaviour to avoid having a collision. As many as one new driver in five has some kind of collision in their first year of driving. This code provides advice to help you get through as safely as possible the first 12 months after passing the driving test — a period when you are most vulnerable.

- Many of the worst collisions happen at night. Between midnight and 6 am is a time of high risk for new drivers. Avoid driving then unless it's really necessary.
- If you are driving with passengers, you are responsible for their safety. Don't let them distract you or encourage you to take risks. Tell your passengers that you need to concentrate if you are to get to your destination safely.
- Never show off or try to compete with other drivers, particularly if they are driving badly.
- Don't drive if you have consumed any alcohol or taken drugs. Even over-the-counter medicines can affect your ability to drive safely — read the label to see if they may affect your driving.
- Make sure everyone in the car is wearing a seat belt throughout the journey.
- Keep your speed down. Many serious collisions happen because the driver loses control, particularly on bends.
- Most new drivers have no experience of driving high-powered or sporty cars. Unless you have learnt to drive in such a vehicle you need to get plenty of experience driving on your own before driving a more powerful car.
- Driving while uninsured is an offence (see below).

REMEMBER that for 12 months after you obtain your full licence you **MUST** display an R-plate on the front and back of your vehicle and **MUST NOT** exceed a speed of 50 mph.

#### **Documents**

Driving licence. You **MUST** have a valid driving licence for the category of motor vehicle you are driving.

[Road Traffic Act 1985, Schedule 3, para 1]

Insurance. To use a motor vehicle on the road, you **MUST** have a valid insurance policy. This **MUST** at least cover you for injury or damage to a third party while using that motor vehicle. Before driving any motor vehicle, make sure that it has this cover for your use or that your own insurance provides adequate cover. You **MUST**

**NOT** drive a motor vehicle without insurance. Also, be aware that even if a road traffic incident is not your fault, you may still be held liable by insurance companies.

[Road Traffic Act 1985, Schedule 5, para 1]

In addition to being liable to the penalties for uninsured driving a suspected offender may have his or her vehicle seized by the police. If convicted, the person may find that the vehicle is ordered by the court to be sold or forfeited.

[Road Traffic Act 1985, ss. 47A to 47D and Schedule 5, para 1]

The types of insurance cover available are —

- third-party, which is often the cheapest form of insurance and is the minimum cover required by law. It covers anyone you might injure or whose property you might damage. It does not cover damage to your own motor vehicle or injury to yourself.
- third-party, fire and theft, which is similar to third-party but also covers you against your motor vehicle being stolen or damaged by fire.
- comprehensive, which is the most expensive but the best insurance. Besides covering other persons and property against injury or damage, it covers damage to your own motor vehicle up to the market value of the vehicle and also personal injury to yourself.

Registration certificate. Registration certificates are issued for all motor vehicles used on the road. They describe them (make, model, etc) and give details of the registered keeper. You **MUST** notify the Licensing Office of the Department of Infrastructure as soon as possible when you buy or sell a motor vehicle or if you change your name or address.

[Licensing and Registration of Vehicles Regulations 2004, regs 8 and 9]

Vehicle Duty. All motor vehicles used or kept on public roads **MUST** display a vehicle licence (tax disc) at all times.

[Licensing and Registration of Vehicles Act 1985, s. 6(4), and Licensing and Registration of Vehicles Regulations 2004, reg 23]

Production of documents. You **MUST** be able to produce your driving licence and valid insurance certificate if required by a police constable to do so. If you cannot, you **MUST** produce them at a nominated police station within five days.

[Road Traffic Act 1985, ss. 42 and 43]

# **ANNEX 11**

## **VEHICLE MAINTENANCE, SAFETY AND SECURITY**

### **Vehicle maintenance and safety**

Take special care that lights, brakes, steering, exhaust system, seat belts, demisters, wipers and washers are all working. Also —

- lights, indicators, reflectors, and number plates **MUST** be kept clean and clear
- windscreens and windows **MUST** be kept clean and free from obstructions to vision
- lights **MUST** be properly adjusted to prevent dazzling other road users. Extra attention needs to be paid to this if the vehicle is heavily loaded
- exhaust emissions **MUST NOT** be excessive
- ensure your seat, seat belt, head restraint and mirrors are adjusted correctly before you drive
- ensure that items of luggage are securely stowed.

[Road Vehicles Lighting Regulations 1989, regs 23 and 27, and Road Vehicles (Maintenance and Use) Regulations 2012, regs 8 and 24]

Warning displays. Make sure that you understand the meaning of all warning displays on the vehicle instrument panel. Do not ignore warning signs — they could indicate a dangerous fault developing.

- When you turn the ignition key, warning lights will be illuminated but will go out when the engine starts (except the handbrake warning light). If they do not, or if they come on while you are driving, stop and investigate the problem as you could have a serious fault.
- If the charge warning light comes on while you are driving, it may mean that the battery isn't charging. This should also be checked as soon as possible to avoid loss of power to lights and other electrical systems.

Window tints. You **MUST NOT** use a vehicle with excessively dark tinting applied to the front windscreen or to the glass in any front window on either side of the driver. Window tinting applied during manufacture complies with the Visual Light Transmittance (VLT) standards. There are no VLT limits for rear windscreens or rear passenger windows.

[Road Vehicles (Construction, Equipment and Weights) Regulations 2012, reg 15]

Tyres. Tyres **MUST** be correctly inflated to the vehicle manufacturer's specification for the load being carried. Always refer to the vehicle's handbook or data. Tyres **MUST** also be free from certain defects, among which are —

- any cut in excess of 25 mm or 10% of the section width of the tyre, whichever is the greater, measured in any direction on the outside of the tyre and deep enough to reach the ply or cord,
- any lump, bulge or tear caused by the separation or partial failure of the tyre's structure,
- any exposure of the ply or cord,
- any part where the base of any groove which showed in the tread pattern of an original, recut or retreaded tyre is not clearly visible.

Cars, dual-purpose vehicles, light vans and light trailers **MUST** have a tread depth of at least 1.6 mm across the central three-quarters of the breadth of the tread and in a continuous band around the entire outer circumference of each tyre.

Motorcycles, heavier goods vehicles and larger passenger-carrying vehicles **MUST** have either —

- a tread depth of at least 1 mm across three-quarters of the breadth of the tread and in a continuous band around the entire outer circumference of each tyre, or
- if the tread pattern does not extend to three-quarters of the breadth of tread, a tread depth of 1 mm across the entire tread pattern.

Mopeds should have visible tread.

Be aware that some vehicle defects can attract penalty points.

[Road Vehicles (Maintenance and Use) Regulations 2012, regs 27 and 28]

If a tyre bursts while you are driving, try to keep control of your vehicle. Grip the steering wheel firmly and allow the vehicle to roll to a stop at the side of the road.

If you have a flat tyre, stop as soon as it is safe to do so. Only change the tyre if you can do so without putting yourself or others at risk — otherwise call a breakdown service.

Tyre pressures. Check weekly. Do this before your journey, when tyres are cold. Warm or hot tyres may give a misleading reading.

Your brakes and steering will be adversely affected by under-inflated or over-inflated tyres. Excessive or uneven tyre wear may be caused by faults in the braking or suspension systems or by wheels which are out of alignment. Have these faults corrected as soon as possible.

Fluid levels. Check the fluid levels in your vehicle at least weekly. Low brake fluid may result in brake failure and a crash. Make sure you recognise the low fluid warning lights if your vehicle has them fitted.

Before winter. Ensure that the battery is well maintained and that there are appropriate anti-freeze agents in your radiator and windscreen bottle.

Other problems. If your vehicle —

- pulls to one side when braking, it is most likely to be a brake fault or incorrectly inflated tyres. Consult a garage or mechanic immediately
- continues to bounce after pushing down on the front or rear, its shock absorbers are worn. Worn shock absorbers can seriously affect the operation of a vehicle and should be replaced
- smells of anything unusual such as burning rubber, petrol or an electrical fault, investigate immediately. Do not risk a fire.

Overheated engines or fire. Most engines are water-cooled. If your engine overheats you should wait until it has cooled naturally. Only then remove the coolant filler cap and add water or other coolant.

If your vehicle catches fire, get the occupants out of the vehicle quickly and to a safe place. Do not attempt to extinguish a fire in the engine compartment as opening the bonnet will make the fire flare. Call the fire brigade.

Petrol stations/fuel tank/fuel leaks. Ensure that, when filling up your vehicle's tank or any fuel cans you are carrying, you do not spill fuel on the forecourt. Any spilled fuel should be immediately reported to the petrol station attendant. Diesel spillage is dangerous to other road users, particularly motorcyclists, as it will significantly reduce the level of grip between the tyres and road surface. Double-check for fuel leaks and make sure that —

- you do not overfill your fuel tank
- the fuel cap is fastened securely
- the seal in the cap is not torn, perished or missing
- there is no visual damage to the cap or the fuel tank.

Emergency fuel caps, if fitted, should form a good seal.

Never smoke, or use a mobile phone, on the forecourt of petrol stations as these are major fire risks and could cause an explosion.

## **Vehicle security**

When you leave your vehicle you should —

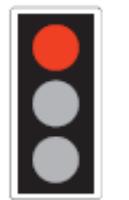
- remove the ignition key and engage the steering lock
- lock the car, even if you only leave it for a few minutes
- close the windows completely
- never leave children or pets in an unventilated car
- take all contents with you or lock them in the boot. Remember that, for all a thief knows, a carrier bag may contain valuables

- never leave vehicle documents in the car.

For extra security fit an anti-theft device such as an alarm or immobiliser. If you are buying a new car it is a good idea to check the level of built-in security features. Consider having your registration number etched on all your car windows. This is a cheap and effective deterrent to professional thieves.

## ANNEX 12

# LIGHT SIGNALS CONTROLLING TRAFFIC IN THE ISLE OF MAN OR GREAT BRITAIN



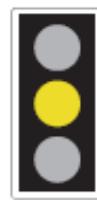
RED means 'Stop'. Wait behind the stop line on the carriageway



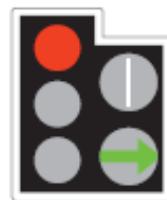
RED AND AMBER also means 'Stop'. Do not pass through or start until GREEN shows



GREEN means you may go on if the way is clear. Take special care if you intend to turn left or right and give way to pedestrians who are crossing



AMBER means 'Stop' at the stop line. You may go on only if the AMBER appears after you have crossed the stop line or are so close to it that to pull up might cause an accident



A GREEN ARROW may be provided in addition to the full green signal if movement in a certain direction is allowed before or after the full green phase. If the way is clear you may go but only in the direction shown by the arrow. You may do this whatever other lights may be showing. White light signals may be provided for trams

### Flashing red lights

Alternately flashing red lights mean  
**YOU MUST STOP**

At level crossings, lifting bridges, airfields, fire stations, etc.



### Motorway signals



You **MUST NOT** proceed further in this lane



Change lane



Reduced visibility ahead



Lane ahead closed



Temporary maximum speed advised and information message



Leave motorway at next exit

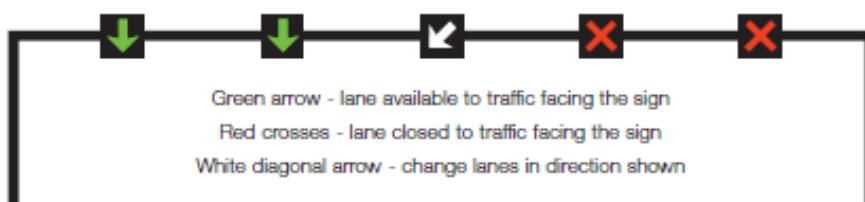


Temporary maximum speed advised



End of restriction

### Lane control signals



## **ANNEX 13**

# **FIRST AID ON THE ROAD**

In the event of an incident you can do a number of things to help, even if you have had no training.

### **1. Deal with danger**

Further collisions and fire are the main dangers following a crash. Approach any vehicle involved with care. Switch off all engines and, if possible, warn other traffic. Stop anyone from smoking.

### **2. Get help**

Try to get the assistance of bystanders. Get someone to call the appropriate emergency services as soon as possible. They will need to know the exact location of the incident and the number of vehicles involved.

### **3. Help those involved**

DO NOT move casualties still in vehicles unless further danger is threatened. DO NOT remove a motorcyclist's helmet unless it is essential. Remember the casualty may be suffering from shock. DO NOT give them anything to eat or drink. DO try to make them warm and as comfortable as you can, but avoid unnecessary movement. DO give reassurance confidently and try not to leave them alone or let them wander into the path of other traffic.

### **4. Provide emergency care**

Remember the letters D R A B C:

D: Danger — check that you are not in danger.

R: Response — try to get a response by asking questions and gently shaking their shoulders.

A: Airway — the airway should be clear and kept open. Place one hand on the forehead, two fingers under the chin and gently tilt the head back.

B: Breathing — normal breathing should be established. Once the airway is open, check breathing for up to 10 seconds.

C: Compressions — if they are not breathing normally, compressions should be administered to maintain circulation. Place two hands in the centre of the chest and press down 4-5 cm at a rate of 100/minute. You may need only one hand for a child. Give 30 chest compressions. Then tilt the head back gently, pinch the casualty's nostrils together, and place your mouth over theirs. Give two breaths, each lasting one second (use gentle breaths for a small child).



If the casualty is unconscious and breathing,  
place them in the recovery position until  
medical help arrives

## Bleeding

First check for anything that may be in the wound, such as glass. If there is nothing embedded, apply firm pressure over the wound. Take care not to press on the object — build up padding on either side of it. Fasten a pad to the wound with a bandage or length of cloth. Use the cleanest material available. If a limb is bleeding but not broken, raise it above the level of the heart to reduce the flow of blood. Any restriction of blood circulation for more than a short time could cause long-term injuries.

## Burns

Try to cool the burn by dousing it with clean, cold water or similar non-toxic liquid for at least 10 minutes. Do not try to remove anything sticking to the burn.

## 5. Be prepared

Always carry a first-aid kit. You could save a life by learning emergency aid and first aid from a qualified organisation such as the St John Ambulance Brigade.

## **ANNEX 14**

### **GREENLANES AND GREENWAY ROADS**

1. There are many miles of ancient unmade roads in the Isle of Man. They are rights of way through the countryside not only for pedestrians but also for other classes of traffic such as horses and their riders, motor cycles, 4 x 4s, and mountain bikes. These unmade roads are known as greenlanes.

2. The surface or narrowness of some greenlanes makes them unsuitable for use by most road vehicles. Some of these have been designated greenway roads and are identified by the green traffic sign "GREENWAY ROAD — BAYR GLASS". Vehicles exceeding an unladen weight of 500 kg are prohibited from using them. For any other vehicles a permit is required unless, as in the case of agricultural, forestry or horticultural vehicles, they are exempt from the prohibition. Application for a permit should be made to the Highways Division, Department of Infrastructure, Sea Terminal Building, Douglas IM1 2RF. Existing greenway roads are listed below.

#### **Greenway roads**

<i>No.</i>	<i>District:</i>	<i>Road</i>	<i>Between</i>	<i>And</i>
G1.	Rushen	Un-named public road	the end of the D24 (Ballaberna Road) north of Surby indicated by a moorland wall and highway gate	its junction with the A36 (Sloc Road and Shoulder Road) indicated by a wall and a highway gate;
G2.	Patrick	U70 (Slieau Whallian Back Road)	its junction with the D47 (Garey Road) indicated by a highway gate	its junction with the C17 (Slieau Whallian Road);
G3.	Malew	U53 (Stoney Mountain Road)	185 metres westwards from the property known as West View on the Stoney Mountain Road. Foxdale	80 metres north-eastwards of its junction with the Stoney Mountain Plantation forestry road, 280 metres eastwards from its junction with the A3 (Castletown to Foxdale Road);

G4.	German	U198 (Cornelly to Kerrowgarrow Road)	its junction with the D55 (Ballacurry Road) at Cornelly Mine, Archallagan	its junction with the U55 (Kennaa Road) at Kerrowgarrow Farm;
G5.	German	D55 (Ballacurry Road)	300 metres south of the former railway crossing at Ballacurry Farm	the property known as Brooklands eastwards from Cornelly Mine at Archallagan, indicated by a highway gate;
G6.	Braddan	U68 (St. Luke's to Brandywell Road); a part of the Millennium Way footpath	95 metres north-east of its junction with the C9 (Ballamoda, Baldwin road) at St. Luke's Church	its junction with the B10 (Beinn y Phott Road) 560 metres north-east of the Glen Crammag bridge;
G7.	Michael	U16 (Ballacurn Road) and all roads which are accessible from it, being roads on Sartfell, Slieau Freoghane, Slieau Dhoo and Slieau Curn which are not macadamized	the junction of U16 with B10 (Beinn y Phott Road) at the Sartfell Plantation, Brandywell indicated by a highway gate south-eastwards of Sartfell	<p>(1) a point 560 metres south-westwards from the junction with the C37 (Druidale Road) indicated by a highway gate north-eastwards of Slieau Dhoo; and</p> <p>(2) the end-on junction with the D14 (Lhergyvreck or "Baltic" Road) 2,200 metres eastwards from the D14 junction with the A3 (Castletown to Ramsey Road) at Kirk Michael, at the common land wall indicated by a highway gate south-west of Slieau Curn; and</p> <p>(3) a point 1,850 metres southwards from the junction at Ballacurn of U16 with the A3 (Castletown to Ramsey Road), at the common land wall indicated by a highway gate north-eastwards of Slieau Curn.</p>

G8.	Ballaugh	U187 (Glendhoo Road)	its junction with the U83 (Ballaugh Plantation Road)	in the property known as Glendhoo or " <i>The Purt</i> " the highway gate at the former Tuck Mill 190 metres southwards of the slab-bridge by which the road crosses the River Dho;
G9.	Ballaugh	U83 (Ballaugh Plantation or Cronagh Road)	A point 230 metres from its junction with the C37 (Druidale Road) at Ravensdale (at the south-western boundary of the forestry car park)	the second junction of the U83 with the C37 (Druidale Road) 740 metres south-westwards from the junction with the U100 (Mount Karrin Road);
G10.	Lezayre	U100 (Ballacuberagh or Mount Karrin Road)	its junction with the A14 (Jurby School to Bungalow Road) in Sulby Glen, north-eastwards of Mount Karrin	its junction with the C37 (Druidale Road) south-westwards of Mount Karrin;
G11.	Lezayre	D10 (Sky Hill Road, including a part of the Millennium Way footpath)	its junction with U77 (Rullick Road) south eastwards of Narradale at Park ne Earkan	its junction with the A10 (Mountain Road) at the Mountain Box (or East Mountain Gate) north-westwards of Clagh Ouyr.

[Greenway Roads (Weight Limit) Order 1994]

3. Users of motor vehicles on greenlanes are reminded that it is necessary to drive with great care and at slow speed because pedestrians, horses and riders, or farm animals may be in the lane at any time. In certain circumstances it may be best to stop your engine and let a horse or other animal pass you by. Also, the surface of the lane may be soft, rough or uncompactated and may have been rutted by vehicles or by flows of water draining from adjacent land. Where a lane is of such a character, motorcyclists need to proceed with particular caution and drivers of 4-wheeled vehicles may find the lane impassable. If a lane bears the sign "Unsuitable for Motors", do not enter it with a motor vehicle.

4. On many greenlanes —

- a white speed-limit notice advises motorcyclists not to exceed 20 mph, and
- a green "finger post" sign depicting a motor cycle and a horse rider may indicate that the lanes are useful ways for motorcyclists and horse riders.

Motorcyclists are advised to ride gently over soft ground.

5. Both pedal cyclists, who should be careful when passing pedestrians, and horse riders are advised to proceed with particular caution if the surface is either soft and uncompactated or hard and rutted.

6. Many greenlanes have one or more gates across them, which may be opened to allow pedestrians, vehicles or horses to pass. A gate must be closed afterwards to prevent sheep or other farm animals from wandering along the road from the area where they are grazing.

7. Road traffic law applies to all vehicles on greenlanes.

## **ANNEX 15**

### **OTHER INFORMATION**

#### **Metric conversions**

A detailed conversion table is provided below:

<b>Miles</b>	<b>Kilometres</b>	<b>Miles</b>	<b>Kilometres</b>
1	1.61	40	64.37
5	8.05	45	72.42
10	16.09	50	80.47
15	24.14	55	88.51
20	32.19	60	96.56
25	40.23	65	104.60
30	48.28	70	112.65
35	56.33		

#### **Useful websites**

Highways Division, Department of Infrastructure: [www.gov.im/transport/highways](http://www.gov.im/transport/highways)

GB Highways Agency — traffic information for visitors by road to England (on road closures, restrictions, diversions etc): [www.highways.gov.uk/traffic/traffic.aspx](http://www.highways.gov.uk/traffic/traffic.aspx).

#### **Further reading**

#### **Best practice**

Further information about good driving and riding practice can be found in the GB Driving Standards Agency books "The Official DSA Guide to Driving — the essential skills" and "The Official DSA Guide to Riding — the essential skills". Information specifically for drivers of large vehicles can be found in "The Official DSA Guide to Driving Goods Vehicles" and "The Official DSA Guide to Driving Buses and Coaches". All these publications may be purchased from the Stationery Office in England or through any good bookseller.

#### **The Blue Badge Scheme**

Information on this scheme can be obtained —

- by accessing the website of the Department of Social Care: [www.gov.im/socialcare/services/disabled/orange.xml](http://www.gov.im/socialcare/services/disabled/orange.xml), or
- by telephoning (01624) 686325, or
- by writing to Blue Badge Scheme, Social Services, 3-4 Hill Street, Douglas, IM1 1EF.

## **Code of Practice for Horse-Drawn Vehicles**

The Code of Practice is available from the GB Department for Transport, Transport Technology and Standards Division 6, 2nd Floor, Great Minster House, 76 Marsham Street, London SW1P 4DR, tel 0207 944 2078.

### **Road works**

A leaflet giving further information on driving through road works can be obtained by telephoning GB Highways Agency Publications, 0870 1226 236, quoting reference number HA113/04.

### **Parking permits**

Information on parking permits can be obtained from the Licensing Office, Department of Infrastructure, Sea Terminal Building, Douglas IM1 2RF.